



**VILLAGE OF BARTLETT
ZONING BOARD OF APPEALS
AGENDA
228 MAIN STREET
December 6, 2018**

7:00 P.M.

- I. Roll Call
- II. Approval of the August 2, 2018 meeting minutes
- III. (#18-06) Bucky's
Variations:
 - a) A 1 foot reduction from the required 50 foot front yard (canopy),
 - b) A 6' reduction from the required 20 foot side yard (carwash),
 - c) Eliminate the required foundation area landscaping (convenience store, west side),
 - d) Reduce the perimeter landscaping on the east property line (required number of large deciduous trees from 9 to 5 (Type 2 Screening)), and
 - e) Eliminate the required trees in 4 of the 6 parking islands**Public Hearing**
- IV. Old Business/New Business
Service Recognition
- V. Adjournment



**Village of Bartlett
Zoning Board of Appeals Minutes
August 2, 2018**

M. Werden called the meeting to order at 7:00 pm.

Roll Call

Present: M. Werden, G. Koziol, B. Bucaro, J. Rasmussen and J. Banno

Absent: L. Hanson

Also Present: A. Zubko, Village Planner

Approval of Minutes

A motion was made to approve the minutes of the July 5, 2018 meeting.

Motioned by: J. Rasmussen

Seconded by: G. Koziol

Roll Call

Ayes: M. Werden, G. Koziol, B. Bucaro, J. Rasmussen and J. Banno

Nays: None

The motion carried.



Village of Bartlett
Zoning Board of Appeals Minutes
August 2, 2018

Case (#18-16) 1439 Snow Drift Circle
Variation
Rear Yard
Public Hearing

The following Exhibits were presented:

- Exhibit A - Picture of Sign**
- Exhibit B - Mail Affidavit**
- Exhibit C - Notification of Publication**

Petitioners, **Deepak and Mona Sharma** were sworn in by **M. Werden**.

Mona Sharma stated she and her husband are requesting a 3'6" variation from the required 30' rear yard to add a sun room, bedroom and a bathroom on to their existing two level home. Legally they can go 7 ft. but they are looking to go 10 ft. **M. Sharma** stated they love their home, the area they just need a little extra room. **M. Werden** stated from what he could see the addition would match the current home. **M. Sharma** stated yes it will blend in with the house. **M. Werden** asked if Staff received any calls from the Public Hearing Notice posted in front of the house. **A. Zubko** stated she did receive one call who said he was going to email a letter but he never did. **B Bucaro** asked **A. Zubko** what the call was about. **A. Zubko** stated the caller just wanted to know what they are building and she explained it was on the rear of the house. The caller had some concerns because the **Shama's** have rented this home in the past but the letter states they will be living in the house now and after the addition. **A. Zubko** stated she assumes the caller didn't have any issues since he never called or wrote back. **M. Werden** asked if anyone from the Board had any questions or comments. **G. Koziol** asked if the two story addition will be under the current deck, a footprint replacement. **M. Sharma** stated yes this is correct. **A. Zubko** stated there will also be a wood deck with a covered patio and a covered porch. **M. Werden** asked if the bay window will stay. **M. Sharma** again stated yes. **G. Koziol** asked if there have been other additions like this in the neighborhood. **A. Zubko** stated she did not look into that, the backyards in this area are quite tiny and she wouldn't be surprised if there weren't any others. **G. Koziol** stated he did go through the neighborhood and didn't see anything like this This will be very unique with a 10% rear yard reduction and seems to be a little close, but if no other neighbors have said anything he believes this to be acceptable. **M. Werden** asked if there will be a need for the stairway. **M. Sharma** stated yes there will still be a stairway. **B Bucaro** asked if there were any previous variances for the stairways coming down towards the rear of the lot. **A. Zubko** stated no the Village does not require a variance for stairs. **G. Koziol** stated once finished it will look more formal than it looks today.

M. Werden asked if **Jeevan Singh**, Architect had anything to add. **J. Singh** stated the existing wood balcony would be removed and the proposed 10'-6" x 34'-3" addition would be constructed. The addition will keep the same character as the entire neighborhood.

M. Werden asked if there were any comments or questions from any members of the Board.

The Public Hearing portion of the meeting was open to the Public. No one came forward.

M. Werden asked for a motion.

G. Koziol made a motion to pass along a positive recommendation to the Village Board to approve Case #18-16, for a rear yard variation at 1439 Snow Drift Circle.



Village of Bartlett
Zoning Board of Appeals Minutes
August 2, 2018

Motioned by: **G. Koziol**
Seconded by: **B. Bucaro**

M. Werden closed the Public Hearing portion of the meeting.

Roll Call

Ayes: **M. Werden, G. Koziol, B. Bucaro, J. Rasmussen and J. Banno**
Nays: None

The motion carried.

M. Werden advised the Petitioner to stay in touch with **A. Zubko** as to when this case will be on the Village Board agenda. **A. Zubko** stated this case would go to Committee August 21, 2018.



**Village of Bartlett
Zoning Board of Appeals Minutes
August 2, 2018**

Old Business/ New Business

A. Zubko stated there may not be a meeting for September. **A. Zubko** also announce that **J. Plonczynski** is retiring as of September 28, 2018 and she was sure there would be something for him to be announced in the near future. The Board members expressed how much they will miss Jim and wished him well.

M. Werden asked if there was a motion to adjourn.

Motioned by: J. Rasmussen

Seconded by: G. Koziol

All in favor.

Motion Carried.

The meeting was adjourned at 7:12 P.M.

COMMUNITY DEVELOPMENT MEMORANDUM

18-216

DATE: November 29, 2018
TO: The Chairman and Members of the Zoning Board of Appeals
FROM: Angela L Zubko, ^{KZ}Community Development Planner
RE: **(#18-06) Bucky's**

PETITIONER

Richard McMahon on behalf of Buchanan Energy

SUBJECT SITE

West side of Route 59, across from Norwood Lane

REQUESTS

Variations:

- a) A 1 foot reduction from the required 50 foot front yard (canopy),
- b) A 6' reduction from the required 20 foot side yard (carwash),
- c) Eliminate the required foundation area landscaping (convenience store, west side),
- d) Reduce the perimeter landscaping on the east property line (required number of large deciduous trees from 9 to 5 (Type 2 Screening)), and
- e) Eliminate the required trees in 4 of the 6 parking islands

SURROUNDING LAND USES

	<u>Land Use</u>	<u>Comprehensive Plan</u>	<u>Zoning</u>
Subject Site	Vacant	Commercial	R-3*
North	Single Family	Commercial	R-3*
South	Commercial	Commercial	B-3 PUD
East	Commercial	Commercial	B-3
West	Single Family	Suburban Residential	R-3*

*Unincorporated DuPage County- Single Family

DISCUSSION

1. The Petitioner is requesting to **Annex** two vacant parcels consisting of 2.18 acres and to **Rezone** the subject property (upon annexation) from the ER-1 (Estate

Residence) Zoning District to the B-3 (Neighborhood Shopping) Zoning District. *(The Annexation will be discussed by the Village Board at a future date and the Rezoning will be discussed by the Plan Commission at their meeting on December 13, 2018).*

2. The Petitioner is also requesting **Preliminary Site Plan Review** for a proposed 5,520 square foot convenience store to accompany an automotive service station with 10 gas pumps (20 fueling stations). A 3,285 square foot carwash is also proposed along the southern portion of the subject property. *(The Preliminary Site Plan Review will be discussed by the Plan Commission at their meeting on December 13, 2018).*
3. The proposed convenience store would be oriented towards Route 59 and would consist primarily of 8"x 8"x16" CMU block in two colors, terra cotta and a cream color along the bottom on all four elevations with a stone veneer on the columns at the three entrances. Metal canopies would add architectural elements over each of the windows. The tallest portions of the building, consisting of the three towers, would be 26'-8". The average height would be 21'-4", meeting the Zoning Code requirement of 25'. The carwash will architecturally match the convenience store with CMU block and will be 18' in height, also meeting the Zoning Code requirement of 25'.

The Staff has taken an inventory of the recently constructed buildings along Route 59 and most consist of brick or stone facades (Walgreens, McDonald's, Tokyo Steakhouse, Wendy's, Home Depot Outlot (i.e. Starbucks), Stearn's Crossing Shopping Center, Oberweis, Apply Valley Shopping Center, First American Bank and the Fifth Third Bank. As previously stated, the Petitioner is proposing to utilize CMU block. Staff was originally shown elevations that depicted the building would be brick. Staff is fine if CMU is utilized along the rear elevation similar to what was approved for Dominick's, the Bartlett Square Shopping Center and Jewel (tinted cast concrete on rear). Staff has requested the Petitioner modify the elevations to brick or face brick to match the former Dominick's Center, Bartlett Square and Jewel.

4. The Petitioner has requested one curb cut along Route 59 for access to the automotive service station which would be located directly across from Norwood Lane. The Village approached the Petitioner regarding the installation and associated costs of a proposed traffic signal at this location. **The Village Staff discussed with the Petitioner a 75%/25% split of the costs; with the Petitioner paying the 75% share. Discussions continue on this subject.** (Route 59 is under the jurisdiction of Illinois Department of Transportation (IDOT) and the Petitioner has been in contact with IDOT regarding this access and a traffic control signal.) The Village has a preliminary cost estimate of \$620,545 from 2015 when this issue was first addressed with the Village Board and the residents in the area. (In this 2015 estimate, the costs for the Petitioner's share would be \$465,408.75, while

the Village's share would be \$155,135.25.) **A revised cost estimate would be provided during the Final Site Plan Review.**

5. It has been Bartlett's policy when reviewing commercial site plans adjacent to one another that the number of curb cuts be minimized and a cross-access easement be utilized between adjoining properties. This encourages the most optimum traffic movements and circulation patterns between commercial entities while providing safe access onto public roadways. The Preliminary Site Plan as proposed does not include a cross-access easement.
6. The Site Plan identifies 59 parking stalls including 2 handicapped accessible stalls. This exceeds the Zoning Ordinance requirement of 39. The Zoning Ordinance requires the number of stacking spaces provided to be "equal in number to 5 times the maximum capacity of the automobile laundry for each wash rack, plus 1 parking space for each 2 employees." A total of 15 stacking spaces would be required to meet the Zoning Ordinance. The Site Plan provides stacking for fifteen (15) vehicles in accordance with the Zoning Ordinance.
7. The Petitioner is proposing to install two 8' tall retaining walls along with an 8' tall vinyl stone fence along the western property line. A 6' tall vinyl fence is also proposed along a portion of the northern property line. As shown on the attached cross section, the subject property is approximately 19' lower than the residentially zoned properties to the west.
8. The Petitioner is requesting a **Plat of Consolidation** to combine the two lots, currently unincorporated, into one zoning lot. Once recorded this lot will have one PIN (parcel identification number). *(The Plat of Consolidation will be discussed by the Plan Commission at their meeting on December 13, 2018).*
9. The Petitioner is also requesting the following **Variations**:
 - a. A 1 foot reduction from the required 50 foot front yard (canopy),
 - b. A 6' reduction from the required 20 foot side yard (carwash),
 - c. Eliminate the required foundation area landscaping (convenience store, west side),
 - d. Reduce the perimeter landscaping on the east property line (required number of large deciduous trees from 9 to 5 (Type 2 Screening)), and
 - e. Eliminate the required trees in 4 of the 6 parking islands

The reduction in the **front yard setback** for the gas station canopy request is consistent with other variations granted for canopies along Route 59 (BP Gas Station - 30' variation, Walgreens - 7.9' variation) and front yard variations granted along Route 59 (primarily for parking).

The reduction in the required **side yard** setback for the carwash is consistent with another variation approved along Route 59 (KinderCare - 4' variation).

The request to eliminate the required **foundation landscaping** (rear elevation) has only been approved once; for Sonic. (The variation granted eliminated the east and west foundation landscaping to allow space for servers to deliver the food orders to parked vehicles.)

The request to reduce the required number of large trees within the 20' wide **perimeter landscaping** from 9 to 5 along Route 59 (east property line) has never been granted by the Village. Sonic was granted a variation in 2016 to eliminate the perimeter landscaping along Stearns 8 years after it was originally approved with the required landscaping but this was a result of the of the right-of-way taking for the Stearns Road intersection improvements.

The request to eliminate the required trees in 4 of the 6 **parking islands** on the site has never been granted by the Village. Ashton Gardens was granted a variation to allow one tree per double island rather than the required two trees, but never to eliminate the trees altogether.

10. **Special Use Permits** are being requested to allow for an Automobile Service Station (Gas Station) and a carwash. The Petitioner has designed the carwash to have the dryer portion of the tunnel wash furthest away from the residential uses to the west. There are 4 vacuums proposed north of the carwash. The retaining walls and proposed 8' tall fence along the west side of the subject property should serve as a sound barrier to minimize any noise emitted from the carwash and vacuums. (Decibel levels would be provided at the time of a Final Site Plan submittal.) *(The Special Use requests will be discussed by the Plan Commission at their meeting on December 13, 2018).*
11. Internal vehicular circulation for the carwash would primarily consist of a counter-clockwise pattern with all vehicles queuing behind the convenience store and entering the carwash along the west side. Vehicles would travel through the tunnel wash and exit the building from the east side. Vehicles would then either exit the site via Route 59 or proceed to the gas pumps or convenience store.
12. The Petitioner is also requesting a **Special Use Permit** to allow outdoor sales and storage including propane and seasonal sale items. These areas are located directly in front (east) of the convenience store.
13. The Petitioner is requesting a **Special Use Permit** for Package Liquor Sales, including beer, wine and liquor. This would be the first service/gas station permitted to sell package liquor in the Village.
14. Underground stormwater will be located below the southern parking lot between the convenience store and the carwash. The Preliminary Engineering

Plans are being reviewed by the Village Engineer and Final Engineering, including stormwater calculations, will be required during their Final Site Plan review process.

15. A Traffic Study, prepared by Quigg Engineering Inc., was reviewed by the Village's Traffic Consultant, Gewalt, Hamilton Associates, Inc. and IDOT. In summary, Quigg Engineering states that **"a traffic signal is warranted at the intersection of IL 59 and Norwood Lane."** The Village's Traffic Consultant concurs with the Traffic Study stating, **"a traffic signal is warranted at the IL Route 59 intersection with Norwood Lane and the proposed site access."**
16. The Village's Future Land Use Plan identifies this area for commercial uses.

RECOMMENDATION

According to the provisions of the Zoning Ordinance, the Zoning Board of Appeals should render a decision based upon the following:

- A. That the particular physical surroundings, shape or topographical condition of the specific property involved would result in a particular hardship upon the owner, as distinguished from a mere inconvenience, if the strict letter of the regulations were carried out.
- B. That conditions upon which the petition for variations are based are unique to the property for which the variation is sought and are not applicable, generally, to other property within the same zoning classifications.
- C. That the purpose of the variations are not based exclusively upon a desire to make money out of the property.
- D. That the alleged difficulty or hardship is caused by the provision of this Title and has not been created by any person presently having an interest in the property.
- E. That the granting of the variations will not be detrimental to the public welfare or injurious to other property or improvements in the neighborhoods in which the property is located.
- F. That the proposed variations will not impair an adequate supply of light and air to adjacent property, or substantially increase the congestion in the public streets, or increase the danger of fire, or endanger the public safety, or substantially diminish or impair property values within the adjacent neighborhood.
- G. That the granting of the variances requested will not confer on the applicant any special privilege that is denied by the provisions of this Title to other lands, structures or buildings in the same district.

CD Memo 18-216
November 29, 2018
Page 6

Variations shall be recommended only if the evidence, in the judgment of the Board of Appeals, sustains all the conditions enumerated above.

Background information is attached for your review and consideration.

alz/attachments

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Buck's Inc.

2500 BRICKVALE ROAD, ELK GROVE VILLAGE, IL 60007
(402) 718-6724
RMcMahon@BuchananEnergy.com

19 October 2018

President Kevin Wallace and Members of the Board of Trustees
Village of Bartlett
228 South Main Street
Bartlett, Illinois 60103

Dear President Wallace;

Buck's Inc., as the development arm of Buchanan Energy (N), LLC, is applying to annex 2 vacant unincorporated lots into the Village of Bartlett and rezone them to B-3 with Special Uses for Car Wash, Automobile Service Station, Package Liquor Sales, and Outdoor Sales of seasonal merchandise and propane.

Putting the vacant lots at the intersection of Il 59 and Norwood under contract began in August 2016. They are part of the 9 lot residentially restricted Williamsburg Woods Subdivision in the County. To move ahead I had to negotiate releases of the residential use restriction with the owners of 6 of the 7 remaining lots (the ComEd lot release is not needed) and obtain contractual approval of the site plan from each of these landowners.

We feel the location will be successful because of its position as the first fuel sales location across the Cook County line. Once the facility is built, we will review and consider retiring the Mobil we own on the southwest corner of the intersection of IL 59 and Sterns. This old location, built in the 1980's is outdated, and the IDOT intersection widening including installation of barrier medians down the center of both roads makes this existing site unsuitable for the future. We have preliminary approval from IDOT for access onto IL 59 directly across from Norwood and have hired a Springfield, Illinois traffic firm to advance our request for a traffic signal warrant from IDOT. We would build with or without that warrant but will continue to press the request which may take over a year to obtain.

Our 7,000 s.f. c-store is the same as the stores we recently opened in Glendale Heights and Wheaton and which have been approved at North and Swift in the County, and at Roosevelt and Lawler on land we annexed into Glen Ellyn. This large store is designed to allow future interior design flexibility when rapid electric car charging technology is finally settled and motorists will have to wait 20 to 30 minutes while their vehicles are charging.

In addition to the additional sales tax and property tax revenue we would bring into the Village, we are planning to connect the water system from Norwood under IL 59 to Bartlett Commons.

FOR THE COMPANY:



Richard J. McMahon

RECEIVED
COMMUNITY DEVELOPMENT

OCT 19 2018

VILLAGE OF
BARTLETT



VILLAGE OF BARTLETT DEVELOPMENT APPLICATION

For Office Use Only
Case # 2018-06
RECEIVED
COMMUNITY DEVELOPMENT
JUN 04 2018
(revised) VILLAGE OF
BARTLETT

PROJECT NAME 931.02 Bucky's Annexation

PETITIONER INFORMATION (PRIMARY CONTACT)

Name: Richard McMahon

Street Address: 2500 Brickvale Drive

City, State: Elk Grove Village, IL

Zip Code: 60007

Email Address: [REDACTED]

Phone Number: [REDACTED]

Preferred Method to be contacted: Email

PROPERTY OWNER INFORMATION

Name: Two Owners (See Attached)

Street Address: _____

City, State: _____

Zip Code: _____

Phone Number: 402-718-6724

OWNER'S SIGNATURE: _____ **Date:** _____
(OWNER'S SIGNATURE IS REQUIRED or A LETTER AUTHORIZING THE PETITION SUBMITTAL.)

ACTION REQUESTED (Please check all that apply)

- Annexation
 - PUD (preliminary)
 - PUD (final)
 - Subdivision (preliminary)
 - Subdivision (final)
 - Site Plan (please describe use: commercial, industrial, square footage):
Neighborhood Shopping- 7,144 s.f. c-store with ancillary use Car Wash
 - Unified Business Center Sign Plan
 - Other (please describe) Consolidate 2 lots into 1 lot.
- Text Amendment
 - Rezoning ER-1 to B-3
 - Special Use for: Car Wash, Automobile Service Station, Package Liquor, outdoor sales
 - Variation: Sign & Canopy setbacks
and landscaping

SIGN PLAN REQUIRED?

(Note: A Unified Business Center Sign Plan is required for four or more individual offices or businesses sharing a common building entrance or private parking lot.)

PROPERTY INFORMATION

Common Address/General Location of Property: West side of Sutton Rd. at Norwood Ln.

Property Index Number ("Tax PIN"/"Parcel ID"): 01-04-401-021 and 01-04-401-027

Zoning: Existing: ER-1
(Refer to Official Zoning Map)

Land Use: Existing: Vacant

Proposed: B-3

Proposed: Commercial

Comprehensive Plan Designation for this Property: Commercial
(Refer to Future Land Use Map)

Acreage: 2.182

For PUD's and Subdivisions:

No. of Lots/Units: _____

Minimum Lot: Area _____ Width _____ Depth _____

Average Lot: Area _____ Width _____ Depth _____

APPLICANT'S EXPERTS (If applicable, including name, address, phone and email)

Attorney Richard McMahon, Buck's Inc.

2500 Brickvale Rd., Elk Grove Village, IL 60007

Engineer Steve Kudwa, Craig Knoche & Associates

24 N. Bennett, Geneva, IL 60134

Other _____

FINDINGS OF FACT (Standards)

The Village of Bartlett Zoning Ordinance requires that certain findings of fact, or standards, must be met before a special use permit, variation, site plan or planned unit development may be granted. Each application for a hearing before the Plan Commission or Zoning Board of Appeals for a special use, variation, site plan or planned unit development must address the required findings of fact for each particular request. The petitioner should be aware that he or she must present specific testimony at the hearing with regards to the findings. **(On the following pages are the findings of fact, or standards, to be met. Please respond to each standard, in writing, as it relates to the case.)**

****PLEASE FILL OUT THE FOLLOWING FINDINGS OF FACT AS THEY RELATE TO YOUR PETITION****

Findings of Fact for **Site Plans**: Pages 4-5

Findings of Fact for **Planned Unit Developments**: Pages 6-9

Findings of Fact for **Special Uses**: Page 10

Findings of Fact for **Variations**: Pages 11-12

FINDINGS OF FACT FOR VARIATIONS

Both the Zoning Board of Appeals and the Village Board must decide if the requested variation is in harmony with the general purpose and intent of the Zoning Ordinance and if there is a practical difficulty or hardship in carrying out the strict letter of the regulations of the Zoning Ordinance.

The Zoning Board of Appeals shall make findings based upon evidence presented on the following standards: **(Please respond to each of these standards in writing below as it relates to your case. It is important that you write legibly or type your responses as this application will be included with the staff report for the ZBA and Village Board to review.)**

1. That the particular physical surroundings, shape or topographical condition of the specific property involved would result in a particular hardship upon the owner, as distinguished from a mere inconvenience, if the strict letter of the regulations were carried out.

The shape and topography of the property and the nature of the business make it impossible to strictly adhere to the regulations. Failure to obtain these variations will cause the developer to abandon the project.

2. That conditions upon which the petition for a variation is based are unique to the property for which the variation is sought and are not applicable, generally, to other property within the same zoning classifications.

There are 3 separate but related businesses integrated and conducted on site, the fuel, the c-store with food, and the car wash unlike other single use properties. Each of the 3 functions requires distinct signage to be successful. The landscape and setback variations are due to the unique nature of the property and the type of business being proposed.

3. That the purpose of the variation is not based exclusively upon a desire to make more money out of the property.

The signage variations are based solely upon the fact that there are 3 interdependent businesses conducted on the site.

4. That the alleged difficulty or hardship is caused by the provisions of this Title and has not been created by any person presently having an interest in the property.

The hardships caused by strict application of the ordinances are not caused by any person having a present interest in the property but by the design constraints unique to the property and the proposed business.

5. That the granting of the variation will not be detrimental to the public welfare or injurious to other property or improvements in the neighborhoods in which the property is located.

These variations will not be detrimental to the public welfare or injurious to other property of improvements in the neighborhoods.

6. That the proposed variation will not impair an adequate supply of light and air to adjacent property, or substantially increase the congestion in the public streets, or increase the danger of fire, or endanger the public safety, or substantially diminish or impair property values within the adjacent neighborhood.

These variations shall not impair adequate supply of light and air to adjacent properties or increase the congestion in the streets. They shall not increase the danger of fire, endanger public safety or in any way diminish or impair property values in the adjacent neighborhoods.

7. That the granting of the variance requested will not confer on the applicant any special privilege that is denied by the provisions of this Title to other lands, structures or buildings in the same district.

These variations will not represent any special privilege denied to other land, structures or buildings in the district.

ACKNOWLEDGEMENT

I understand that by signing this form, that the property in question may be visited by village staff and Board/Commission members throughout the petition process and that the petitioner listed above will be the primary contact for all correspondence issued by the village.

I certify that the information and exhibits submitted are true and correct to the best of my knowledge and that I am to file this application and act on behalf of the above signatures.

Any late, incomplete or non-conforming application submittal will not be processed until ALL materials and fees have been submitted.

SIGNATURE OF PETITIONER: Richard J. McMahon

PRINT NAME: Richard J. McMahon *attorney-in-fact*

DATE: 4 June 2018

REIMBURSEMENT OF CONSULTANT FEES AGREEMENT

The undersigned hereby acknowledges his/her obligation to reimburse the Village of Bartlett for all necessary and reasonable expenses incurred by the Village for review and processing of the application. Further, the undersigned acknowledges that he/she understands that these expenses will be billed on an ongoing basis as they are incurred and will be due within thirty days. All reviews of the petition will be discontinued if the expenses have not been paid within that period. Such expenses may include, but are not limited to: attorney's fees, engineer fees, public advertising expenses, and recording fees. Please complete the information below and sign.

NAME OF PERSON TO BE BILLED: Buck's Inc. c/o Richard McMahon

ADDRESS: [REDACTED]

PHONE NUMBER: [REDACTED]

EMAIL: [REDACTED]

SIGNATURE: Richard J. McMahon, attorney-in-fact

DATE: 4 June 2018

Village of Bartlett Development Committee

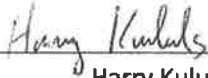
228 South Main Street

Bartlett, Illinois 60103

To Whom It May Concern;

This letter serves as authorization for Buck's Inc. to apply for rezoning and annexation of Lot 9 in Williamsburg Woods Subdivision, DuPage County, Illinois (P.I.N. 01-04-401-027) from R-3 to B-3 Planned Unit Development with a Special Use for an automobile service station, convenience store, and car wash together with any other ancillary requirements needed by Buck's Inc to compliment such uses.

By: The Harry Kulubis Estate Trust Dated December 1, 1988 as Amended



Harry Kulubis

Trustee



Date

Village of Bartlett Development Committee

228 South Main Street

Bartlett, Illinois 60103

To Whom It May Concern;

This letter serves as authorization for Buck's Inc. to apply for rezoning and annexation of Lot 8 in Williamsburg Woods Subdivision, DuPage County, Illinois, (P.I.N. 01-040-401-021) from R-3 to B-3 Planned Unit Development with a Special Use for an automobile service station, convenience store, and car wash together with any other ancillary requirements needed by Buck's Inc to compliment such uses.

By: Beata Burczyc



Beata Burczyc



Date

11
e, and
uses

ZONING/LOCATION MAP

Bucky's

Case #18-06 - Annexation, Rezoning, Special Uses, Variations, Site Plan & Plat of Consolidation



OCT 19 2018

VILLAGE OF BARTLETT

PLAT OF ANNEXATION

TO THE VILLAGE OF BARTLETT, DuPAGE COUNTY, ILLINOIS

BEING

LOT EIGHT AND NINE (EXCEPT THE EAST 10.00 FEET THEREOF, TAKEN IN CONDEMNATION CASE 92ED-4) IN WILLIAMSBURG WOODS, BEING A SUBDIVISION IN THE SOUTHEAST QUARTER OF SECTION FOUR, TOWNSHIP FORTY NORTH, RANGE NINE, EAST OF THE THIRD PRINCIPAL MERIDIAN, ACCORDING TO THE PLAT THEREOF RECORDED NOVEMBER 28, 1966 AS DOCUMENT NUMBER R66-46292, IN THE TOWNSHIP OF WAYNE, IN DUPAGE COUNTY, ILLINOIS.



ANNEXATION CERTIFICATE

STATE OF ILLINOIS }
COUNTY OF DuPAGE } S.S.
THIS IS TO CERTIFY THAT THE PLAT HEREON DRAWN IS A CORRECT REPRESENTATION OF THE TERRITORY ANNEXED TO THE VILLAGE OF BARTLETT, COOK, DUPAGE AND KANE COUNTIES, ILLINOIS BY
ORDINANCE NO. _____
APPROVED ON _____
DATED AT BARTLETT, ILLINOIS THIS _____ DAY OF _____, A.D. 20__

VILLAGE PRESIDENT _____
ATTEST:
VILLAGE CLERK _____

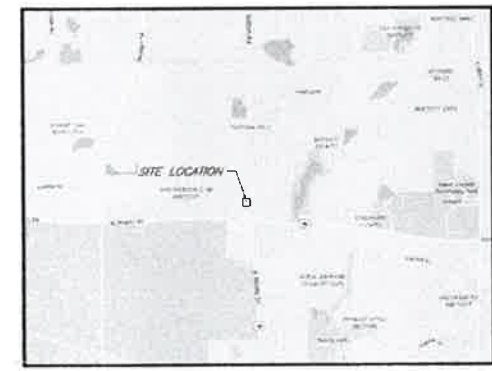
OWNERS CERTIFICATE

STATE OF ILLINOIS }
COUNTY OF DuPAGE } S.S.
THIS IS TO CERTIFY THAT BUCHANAN ENERGY (N), LLC. IS THE OWNER OF RECORD OF THE HEREON DESCRIBED PROPERTY AND THAT AS SUCH OWNER HEREBY CONSENTS TO THE ANNEXATION OF SAID PROPERTY TO THE VILLAGE OF BARTLETT, DuPAGE COUNTY, ILLINOIS.
DATED AT _____ THIS _____ DAY OF _____ 20__
BY: _____
TITLE: PRESIDENT OF MANAGING MEMBER CHICAGO SPE (N), INC.

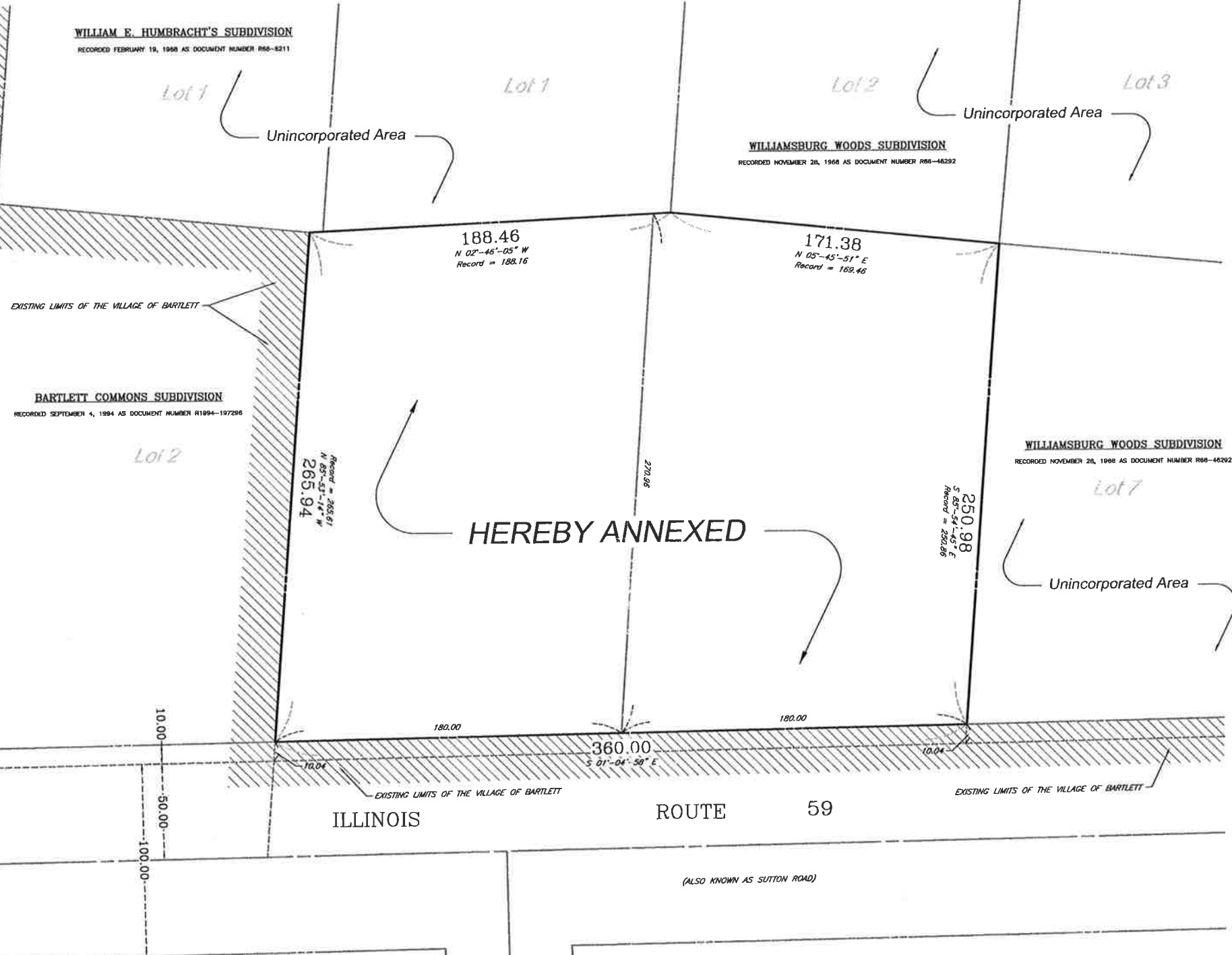
NOTARY CERTIFICATE

STATE OF NEBRASKA }
COUNTY OF DOUGLAS } S.S.
BEFORE ME, A NOTARY PUBLIC QUALIFIED FOR SAID COUNTY AND STATE, PERSONALLY CAME STEVE BUCHANAN, PRESIDENT OF MANAGING MEMBER CHICAGO SPE (N), LLC, KNOWN TO ME TO BE THE IDENTICAL PERSON WHO SIGNED THE FOREGOING INSTRUMENT AND ACKNOWLEDGE THE EXECUTION THEREOF TO BE HIS VOLUNTARY ACT AND DEED ON BEHALF OF BUCHANAN ENERGY (N), LLC.
WITNESS MY HAND AND NOTARY SEAL, ON THIS _____ DAY OF _____ 20__
NOTARY PUBLIC _____

SITE LOCATION MAP (Not to Scale)



WILLIAM E. HUMBRACHT'S SUBDIVISION
RECORDED FEBRUARY 19, 1968 AS DOCUMENT NUMBER R66-8211



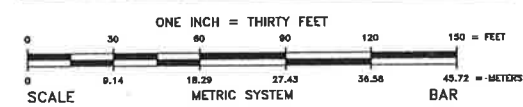
BARTLETT COMMONS SUBDIVISION
RECORDED SEPTEMBER 4, 1994 AS DOCUMENT NUMBER R1994-197296

WILLIAMSBURG WOODS SUBDIVISION
RECORDED NOVEMBER 28, 1966 AS DOCUMENT NUMBER R66-46292

WILLIAMSBURG WOODS SUBDIVISION
RECORDED NOVEMBER 28, 1966 AS DOCUMENT NUMBER R66-46292

HEREBY ANNEXED

AREA	SQUARE FEET	ACRES
LOT 8	47077	1.081
LOT 9	47959	1.101
TOTAL	95046	2.182



METES AND BOUNDS LEGAL DESCRIPTION

THAT : OT EIGHT AND NINE (EXCEPT THE EAST 10.00 FEET THEREOF, TAKEN IN CONDEMNATION CASE 92ED-4) IN WILLIAMSBURG WOODS, BEING A SUBDIVISION IN THE SOUTHEAST QUARTER OF SECTION FOUR, TOWNSHIP FORTY NORTH, RANGE NINE, EAST OF THE THIRD PRINCIPAL MERIDIAN, ACCORDING TO THE PLAT THEREOF RECORDED NOVEMBER 28, 1966 AS DOCUMENT NUMBER R66-46292, IN THE TOWNSHIP OF WAYNE, BOUNDED AND DESCRIBED AS FOLLOWS:

BEGINNING AT THE SOUTHWEST CORNER OF SAID LOT 9; THENCE NORTH 2 DEGREES 46 MINUTES 05 SECONDS WEST ON THE WEST LINE OF SAID LOT 9 A DISTANCE OF 188.46 FT. TO THE SOUTHEAST CORNER OF SAID LOT 8; THENCE NORTH 5 DEGREES 45 MINUTES 51 SECONDS EAST ON THE WEST LINE OF SAID LOT 8 A DISTANCE OF 171.38 FT. TO THE NORTHWEST CORNER OF SAID LOT 8; THENCE SOUTH 86 DEGREES 54 MINUTES 45 SECONDS EAST ON THE NORTH LINE OF SAID LOT 8 A DISTANCE OF 250.98 FT. TO THE WEST LINE OF ILLINOIS ROUTE 59 ACCORDING TO CONDEMNATION CASE 92ED-4; THENCE SOUTH 1 DEGREE 4 MINUTES 58 SECONDS EAST ON SAID WEST LINE A DISTANCE OF 360.00 FT. TO THE SOUTH LINE OF SAID LOT 9; THENCE NORTH 85 DEGREES 53 MINUTES 14 SECONDS WEST ON THE SOUTH LINE OF SAID LOT 9 A DISTANCE OF 265.94 FT. TO THE POINT OF BEGINNING, ALL IN DUPAGE COUNTY, ILLINOIS.

PIN NO.: 01 - 04 - 401 - 010
01 - 04 - 401 - 021

SUBMITTED BY / RETURN TO:
VILLAGE OF BARTLETT
228 S. MAIN STREET
BARTLETT, IL 60103
(630) 837-0800

RECORDER'S CERTIFICATE

STATE OF ILLINOIS }
COUNTY OF DuPAGE } SS
THIS INSTRUMENT WAS FILED FOR RECORD IN THE RECORDER'S OFFICE OF DUPAGE COUNTY, ILLINOIS,
THIS _____ DAY OF _____ 20__ AT _____ O'CLOCK ____ M.
AND RECORDED IN MAP BOOK _____, PAGE _____, AS DOCUMENT NO. _____
BY: _____ COUNTY RECORDER

SURVEYOR'S CERTIFICATE

STATE OF ILLINOIS }
COUNTY OF DuPAGE } SS
THIS IS TO CERTIFY THAT I PAUL N. MARCHESE, AN ILLINOIS REGISTERED PROFESSIONAL LAND SURVEYOR, HAVE PLATTED FROM THE RECORD PROPERTY DESCRIBED AS FOLLOWS:
LOT EIGHT AND NINE (EXCEPT THE EAST 10.00 FEET THEREOF, TAKEN IN CONDEMNATION CASE 92ED-4) IN WILLIAMSBURG WOODS, BEING A SUBDIVISION IN THE SOUTHEAST QUARTER OF SECTION FOUR, TOWNSHIP FORTY NORTH, RANGE NINE, EAST OF THE THIRD PRINCIPAL MERIDIAN, ACCORDING TO THE PLAT THEREOF RECORDED NOVEMBER 28, 1966 AS DOCUMENT NUMBER R66-46292, IN THE TOWNSHIP OF WAYNE, IN DUPAGE COUNTY, ILLINOIS.
AS SHOWN BY THE ANNEXED PLAT FOR THE PURPOSES OF ANNEXATION TO THE VILLAGE OF BARTLETT, ILLINOIS, AND THE ANNEXED PLAT ACCURATELY SHOWS SUCH TERRITORY TO BE ANNEXED, ALL DISTANCES ARE SHOWN IN FEET AND DECIMALS THEREOF, SCALE OF MAP IS 30 FEET TO ONE INCH.
GIVEN UNDER MY HAND AND SEAL AT ROSELLE, ILLINOIS, THIS 12TH DAY OF JULY, 2018

PAUL N. MARCHESE
ILLINOIS PROFESSIONAL LAND SURVEYOR NO. 035-002451
10 MONACO DRIVE
ROSELLE, ILLINOIS 60172
(630) 894-5680
MY CURRENT LICENSE EXPIRES ON NOVEMBER 30, 2018

SHEET: 1 OF 1
ADDRESS: ILLINOIS ROUTE 59 & NORWOOD LANE
BARTLETT, ILLINOIS 60103
SCALE: ONE INCH = THIRTY FEET
ORDER NO.: 16-16108
ORDERED BY: BUCHANAN ENERGY (N) LLC.

COMPARE ALL POINT BEFORE BUILDING BY SAME AND AT ONCE REPORT ANY DIFFERENCE. FOR BUILDING LINE AND OTHER RESTRICTIONS NOT SHOWN HEREON REFER TO YOUR CONTRACT, DEED, TITLE INSURANCE POLICY AND ZONING ORDINANCE. BASIS OF BEARINGS SHOWN HEREON HAVE BEEN ASSUMED, MONUMENTS OR WITNESS POINTS WERE NOT SET AT THE CLIENT'S REQUEST.

DATE	REVISION	MARK
6/25/18	Village Review Comments Added	3
5/1/18	Village Review Comments Added	2
12/14/17	Original Draft Prepared	1

REVISION SCHEDULE

PLAT OF ANNEXATION
Prepared By
MARCHESE AND SONS, Inc.
land - marine - construction surveys
10 Monaco Drive Roselle, Illinois 60172 Phone : (630) 894-5680 FAX : (630) 894-8869

RECEIVED
COMMUNITY DEVELOPMENT
OCT 19 2018
VILLAGE OF
BARTLETT

BUCHANAN'S PLAT OF CONSOLIDATION

of
PART OF THE SOUTHEAST QUARTER OF SECTION FOUR, TOWNSHIP FORTY NORTH, RANGE NINE, EAST OF THE THIRD PRINCIPAL MERIDIAN, IN THE TOWNSHIP OF WAYNE, IN DUPAGE COUNTY, ILLINOIS.



VILLAGE OF BARTLETT CERTIFICATE

STATE OF ILLINOIS)
COUNTY OF DuPAGE) S.S.

APPROVED AND ACCEPTED BY THE PRESIDENT AND BOARD OF TRUSTEES OF THE VILLAGE OF BARTLETT, DUPAGE COUNTY, ILLINOIS

THIS _____ DAY OF _____, A.D. 20____

BY: _____
VILLAGE PRESIDENT

ATTEST: _____
VILLAGE CLERK

OWNER'S CERTIFICATE

STATE OF NEBRASKA)
COUNTY OF DOUGLAS) S.S.

THIS IS TO CERTIFY THAT BUCHANAN ENERGY (N), LLC IS THE LEGAL OWNER OF THE LAND DESCRIBED ON THE SUBJECT PLAT, AND HAS CAUSED THE SAME TO BE SURVEYED AND SUBDIVIDED AS INDICATED THEREON, FOR THE USES AND PURPOSES HEREIN SET FORTH.

ALSO, THIS IS TO CERTIFY THAT THE PROPERTY BEING SUBDIVIDED AFORESAID AND, TO THE BEST OF OWNER'S KNOWLEDGE AND BELIEF, SAID SUBDIVISION LIES ENTIRELY WITHIN THE BOUNDARIES OF THE U-46 SCHOOL DISTRICT.

DATED AT _____ THIS _____ DAY OF _____, 20____

BY: _____
STEVE BUCHANAN

TITLE: PRESIDENT OF MANAGING MEMBER
CHICAGO SPE (N), INC.

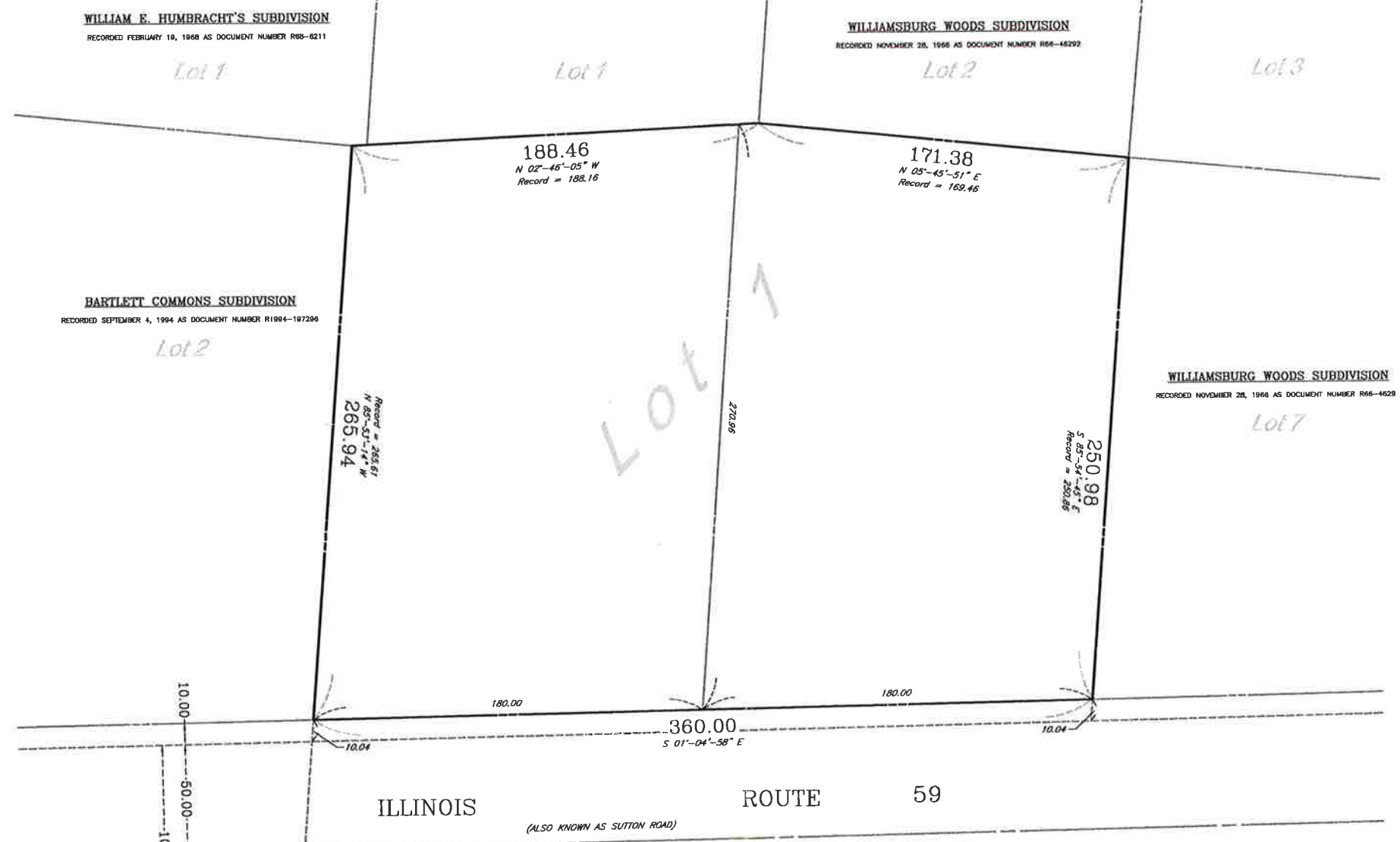
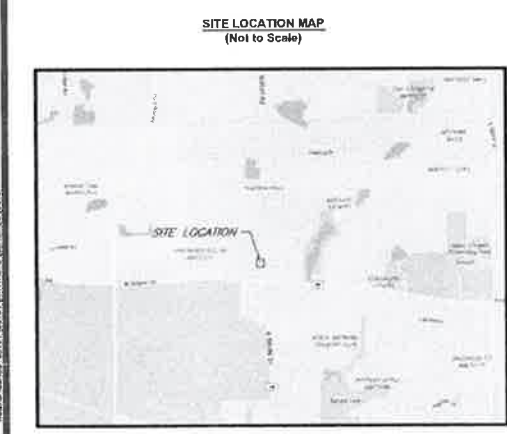
NOTARY CERTIFICATE

STATE OF NEBRASKA)
COUNTY OF DOUGLAS) S.S.

BEFORE ME, A NOTARY PUBLIC QUALIFIED FOR SAID COUNTY AND STATE, PERSONALLY CAME STEVE BUCHANAN, PRESIDENT OF MANAGING MEMBER CHICAGO SPE (N), LLC, KNOWN TO ME TO BE THE IDENTICAL PERSON WHO SIGNED THE FOREGOING INSTRUMENT AND ACKNOWLEDGE THE EXECUTION THEREOF TO BE HIS VOLUNTARY ACT AND DEED ON BEHALF OF BUCHANAN ENERGY (N), LLC.

WITNESS MY HAND AND NOTARY SEAL, ON THIS _____ DAY OF _____, A.D. 20____

NOTARY PUBLIC



METES AND BOUNDS LEGAL DESCRIPTION

THAT LOT EIGHT AND NINE (EXCEPT THE EAST 10.00 FEET THEREOF, TAKEN IN CONDEMNATION CASE 92ED-4) IN WILLIAMSBURG WOODS, BEING A SUBDIVISION IN THE SOUTHEAST QUARTER OF SECTION FOUR, TOWNSHIP FORTY NORTH, RANGE NINE, EAST OF THE THIRD PRINCIPAL MERIDIAN, ACCORDING TO THE PLAT THEREOF RECORDED NOVEMBER 28, 1966 AS DOCUMENT NUMBER R66-46292, IN THE TOWNSHIP OF WAYNE, BOUNDED AND DESCRIBED AS FOLLOWS:

BEGINNING AT THE SOUTHWEST CORNER OF SAID LOT 9; THENCE NORTH 2 DEGREES 46 MINUTES 05 SECONDS WEST ON THE WEST LINE OF SAID LOT 9 A DISTANCE OF 188.46 FT. TO THE SOUTHEAST CORNER OF SAID LOT 8; THENCE NORTH 5 DEGREES 45 MINUTES 51 SECONDS EAST ON THE WEST LINE OF SAID LOT 8 A DISTANCE OF 171.38 FT. TO THE NORTHWEST CORNER OF SAID LOT 8; THENCE SOUTH 85 DEGREES 54 MINUTES 45 SECONDS EAST ON THE NORTH LINE OF SAID LOT 8 A DISTANCE OF 250.98 FT. TO THE WEST LINE OF ILLINOIS ROUTE 59 ACCORDING TO CONDEMNATION CASE 92ED-4; THENCE SOUTH 1 DEGREE 4 MINUTES 59 SECONDS EAST ON SAID WEST LINE A DISTANCE OF 360.00 FT. TO THE SOUTH LINE OF SAID LOT 9; THENCE NORTH 85 DEGREES 53 MINUTES 14 SECONDS WEST ON THE SOUTH LINE OF SAID LOT 9 A DISTANCE OF 265.94 FT. TO THE POINT OF BEGINNING, ALL IN DUPAGE COUNTY, ILLINOIS.

COUNTY CLERK CERTIFICATE

STATE OF ILLINOIS)
COUNTY OF DUPAGE) S.S.

_____, COUNTY CLERK OF DUPAGE COUNTY, ILLINOIS DO HEREBY CERTIFY THAT THERE ARE NO DELINQUENT GENERAL TAXES, NO UNPAID CURRENT TAXES, NO UNPAID FORFEITED TAXES, AND NO REDEEMABLE TAX SALES AGAINST ANY OF THE LAND INCLUDED IN THE ANNEXED PLAT.

I FURTHER CERTIFY THAT I HAVE RECEIVED ALL STATUTORY FEES IN CONNECTION WITH THE CONSOLIDATED PLAT.

GIVEN UNDER MY HAND AND SEAL OF THE COUNTY CLERK AT WHEATON, ILLINOIS

THIS _____ DAY OF _____, A.D. 20____ AT _____ O'CLOCK _____ M.

BY: _____
COUNTY CLERK

ILLINOIS DEPARTMENT OF TRANSPORTATION CERTIFICATE

THIS THIS PLAT HAS BEEN APPROVED BY THE ILLINOIS DEPARTMENT OF TRANSPORTATION WITH RESPECT TO ROADWAY ACCESS PURSUANT OF §2 OF "AN ACT TO REVISE THE LAW IN RELATION TO PLATS," AS AMENDED. A PLAN THAT MEETS THE REQUIREMENTS CONTAINED IN THE DEPARTMENT'S "POLICY ON PERMITS FOR ACCESS DRIVEWAYS TO STATE HIGHWAYS" WILL, BE REQUIRED BY THE DEPARTMENT.

Anthony J. Quigley, P.E.
Region One Engineer

ONE INCH = THIRTY FEET

SCALE METRIC SYSTEM BAR

RECORDER'S CERTIFICATE

STATE OF ILLINOIS)
COUNTY OF DuPAGE) S.S.

THIS INSTRUMENT WAS FILED FOR RECORD IN THE RECORDER'S OFFICE OF DUPAGE COUNTY, ILLINOIS:

THIS _____ DAY OF _____, A.D. 20____ AT _____ O'CLOCK _____ M.

AND RECORDED IN MAP BOOK _____, PAGE _____, AS DOCUMENT NO. _____

BY: _____
COUNTY RECORDER

SURVEYOR'S CERTIFICATE

STATE OF ILLINOIS)
COUNTY OF DuPAGE) S.S.

THIS IS TO CERTIFY THAT I PAUL N. MARCHESE, AN ILLINOIS PROFESSIONAL LAND SURVEYOR, HAVE SURVEYED, SUBDIVIDED AND PLATTED FOR THE OWNERS THEREOF THAT PART OF THE SOUTHEAST QUARTER OF SECTION FOUR, TOWNSHIP FORTY NORTH, RANGE NINE EAST OF THE THIRD PRINCIPAL MERIDIAN, DESCRIBED AS FOLLOWS:

LOT EIGHT AND NINE (EXCEPT THE EAST 10.00 FEET THEREOF, TAKEN IN CONDEMNATION CASE 92ED-4) IN WILLIAMSBURG WOODS, BEING A SUBDIVISION IN THE SOUTHEAST QUARTER OF SECTION FOUR, TOWNSHIP FORTY NORTH, RANGE NINE, EAST OF THE THIRD PRINCIPAL MERIDIAN, ACCORDING TO THE PLAT THEREOF RECORDED NOVEMBER 28, 1966 AS DOCUMENT NUMBER R66-46292, IN THE TOWNSHIP OF WAYNE, IN DUPAGE COUNTY, ILLINOIS.

CONTAINING 95046 SQUARE FEET OR 2.182 ACRES, MORE OR LESS

I FURTHER CERTIFY THAT IRON STAKES HAVE BEEN SET AT ALL LOT CORNERS, POINTS OF CURVATURE AND TANGENCY, EXCEPT WHERE CONCRETE MONUMENTS ARE INDICATED, AND THAT THE PLAT HEREON DRAWN CORRECTLY REPRESENTS SAID SURVEY AND SUBDIVISION. DIMENSIONS ARE GIVEN IN FEET AND DECIMAL PARTS THEREOF.

I FURTHER CERTIFY THAT THE FOREGOING PROPERTY FALLS WITHIN THE CORPORATE LIMITS OF THE VILLAGE OF BARTLETT, AND I FURTHER CERTIFY THAT NO PART OF SAID PROPERTY IS SITUATED WITHIN A FLOOD HAZARD AREA, AS PER NATIONAL FLOOD INSURANCE PROGRAM, FLOOD INSURANCE RATE MAP, COMMUNITY PANEL NUMBER 17043 C 0102 H, EFFECTIVE DATE DECEMBER 16, 2004

GIVEN UNDER MY HAND AND SEAL AT ROSELLE, ILLINOIS, THIS 12TH DAY OF JULY, 2018

PAUL N. MARCHESE
ILLINOIS PROFESSIONAL LAND SURVEYOR NO. 035-002461
10 MONACO DRIVE
ROSELLE, ILLINOIS 60172
(630) 894-5680

MY CURRENT LICENSE EXPIRES ON NOVEMBER 30, 2018

SHEET: 1 OF 1

ADDRESS: ILLINOIS ROUTE 59 & NORWOOD LANE
BARTLETT, ILLINOIS 60103

SCALE: ONE INCH = THIRTY FEET

ORDER NO.: 16-16108

ORDERED BY: BUCHANAN ENERGY (N) LLC.

COMPARE ALL POINT BEFORE BUILDING BY SAME AND AT ONCE REPORT ANY DIFFERENCE. FOR BUILDING LINE AND OTHER RESTRICTIONS NOT SHOWN HEREON REFER TO YOUR CONTRACT, DEED, TITLE INSURANCE POLICY AND ZONING ORDINANCE. BASIS OF BEARINGS SHOWN HEREON HAVE BEEN ASSUMED. MONUMENTS OR WITNESS POINTS WERE NOT SET AT THE CLIENT'S REQUEST.

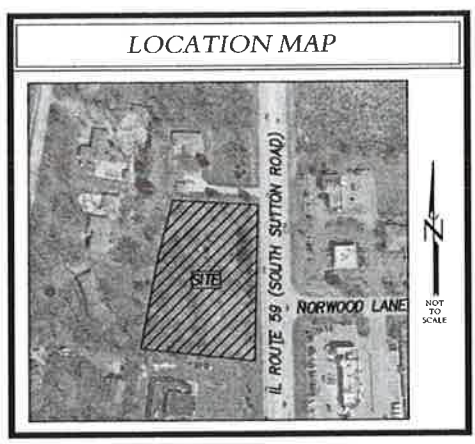
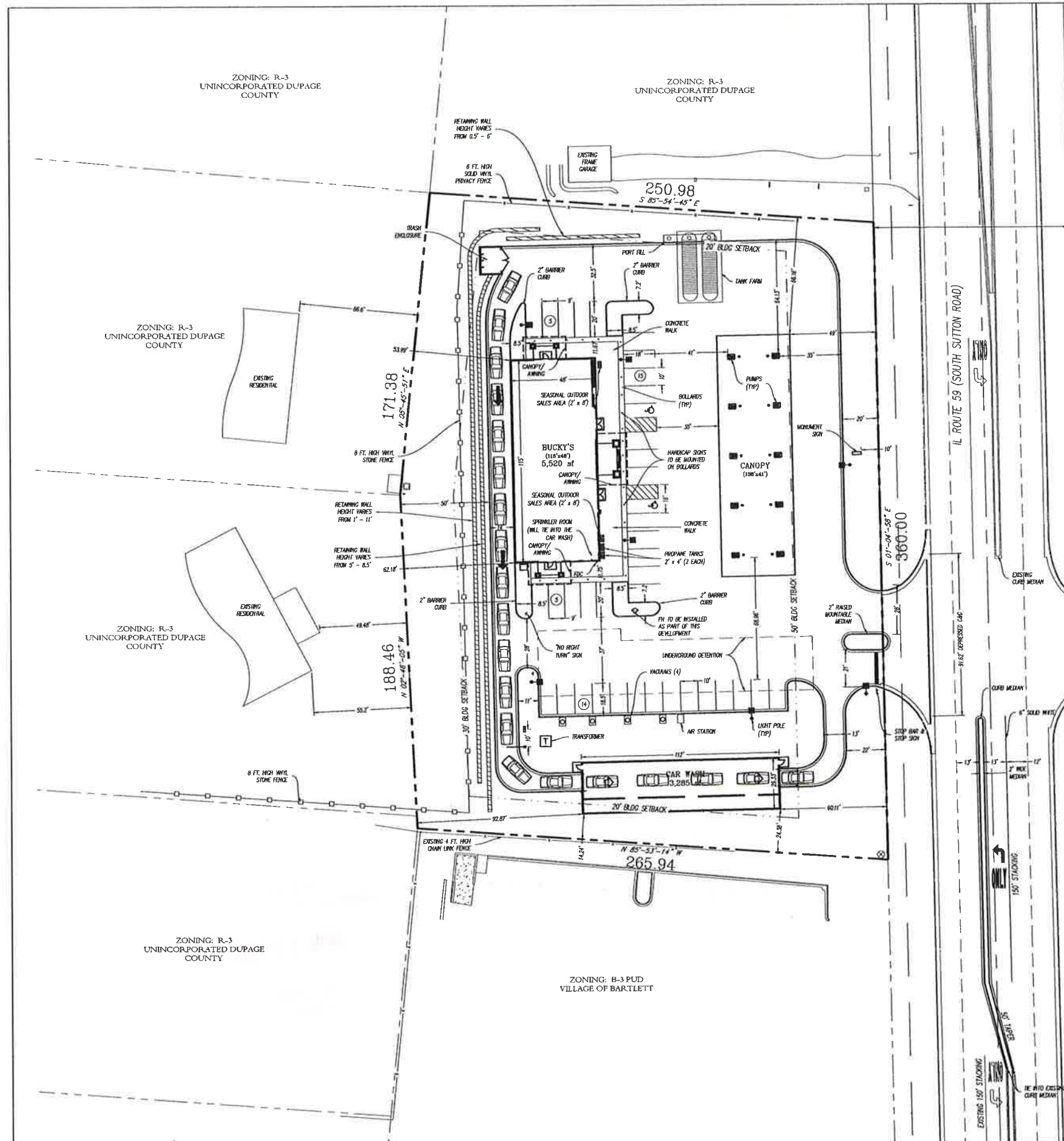
DATE	REVISION	MARK
6/25/18	Village Review Comments Added	3
5/1/18	Village Review Comments Added	2
12/14/17	Original Draft Prepared	1

REVISION SCHEDULE

PLAT OF CONSOLIDATION
Prepared By
MARCHESE AND SONS, Inc.
Design Firm No. 164-002762
land - marine - construction surveys

10 Monaco Drive
Roselle, Illinois 60172
Phone: (630) 894-5680
FAX: (630) 894-8869

ILLINOIS DEPARTMENT OF TRANSPORTATION
 REGION ONE ENGINEER
 ANTHONY J. QUIGLEY, P.E.
 1000 N. WHEELING AVENUE, SUITE 100
 WHEATON, ILLINOIS 60187
 (630) 894-5680
 FAX (630) 894-8869
 WWW.MARCHESEANDSONS.COM



SITE ANALYSIS

SITE
 IL Route 59 & Norwood Lane
 Bartlett, IL
 Proposed Use Convenience Store/Gas Station (Service Station) & Carwash (Automobile Laundry)
 Existing Zoning R-3 (Unincorporated DuPage County)
 Required Zoning B-3 (Village of Bartlett)

Parcel Area	95,046 sf ±2.18 Acres
Building Area	7,144 sf Service Station 3,285 sf Automobile Laundry
Building Height	21' 4" - Building 27' 4" - Tower
Floor Area Ratio Reqd (Max)	0.40
Provided	0.10

SITE

Number of Vacuums	4
Number of Pumps	10
Building Height	21' 4" - Building 27' 4" - Tower
Number of Employees	4
Customer Floor Area	3,358 sf

SETBACKS

	Front Yard (IL. Rte. 59)	Rear Yard	Side Yard (North)	Side Yard (South)
Convenience Store Setback	50'	30'	20'	20'
Provided	159'	54'	86.5'	153.1'
Car Wash Setback	50'	30'	20'	20'
Provided	60.1'	92.9'	304.9'	14.2'
Gas Canopy Setback	50'	30'	20'	20'
Provided	49'	168.8'	66.2'	154.1'
Green Space Setback	20'	12'	12'	5'
Provided	20'	23.8'	12'	14.2'

**Landscape setback for north side yard is based on Alternate B for Type 1 Screening.

PARKING

Quantity Reqd	2 = 1 x 4 / 2 17 = 1 x 3338/200 20 = 2 x 10 39 Required	(1 space for each 2 employees - 4 employees total) (1 space for each 200 sf of customer floor area) (2 spaces for each island of pumps-10 total pumps)
Provided	59 = 37 Standard + 2 ADA + 20 Fueling Spaces	
Stall Size Required	10' x 18' (Standard), 9' x 20' (Standard)	
Provided	10' x 18' (Standard), 9' x 20' (Standard) 16' x 18' (ADA) 10' x 18' (Vacuum Stall)	

AUTOMOBILE LAUNDRY

Quantity Reqd	15 = 5 x 3 15 Required	Automobile Laundry (5 x capacity of car wash)
Provided	15 = 15 Stacking Spaces	

GREEN SPACE

Reqd	15%
Provided	39%

SITE IMPERVIOUS

EXISTING IMPERVIOUS AREA = 0 SF
 PROPOSED IMPERVIOUS AREA = 57,690 SF
 NET NEW IMPERVIOUS AREA = 57,690 SF

VOLUME CONTROL IS PROPOSED PER SECTION 15-64 OF THE DUPAGE COUNTY CSPO.
 TOTAL VOLUME CONTROL PROVIDED IS EQUAL TO 57,690 SF * 1.25', OR 6,000 CU.FT.

SITE NOTES

- ALL DIMENSIONS ARE BACK OF CURB UNLESS OTHERWISE NOTED.
- ALL CURB RADI ARE BACK OF CURB UNLESS OTHERWISE NOTED.
- CONTRACTOR TO VERIFY DIMENSIONS PRIOR TO STARTING WORK AND NOTIFY ENGINEER IF ANY DISCREPANCIES ARE FOUND.
- SIDEWALK AROUND PERIMETER OF BUILDING SHALL BE INTEGRAL CURB AND SIDEWALK EXCEPT AT HANDICAP STALL LOCATIONS WHERE IT IS TO BE WALK/PAVEMENT UNLESS OTHERWISE SPECIFIED ON PLANS.
- CONTRACTOR WILL BE RESPONSIBLE FOR REPAIRING ALL EXISTING PAVEMENT DAMAGED DURING CONSTRUCTION.
- SEE DETAILS FOR BITUMINOUS AND CONCRETE PAVEMENT SECTIONS.
- CONTRACTOR TO PROVIDE TEMPORARY TRAFFIC CONTROL MEASURES DURING CONSTRUCTION OF ENTRANCE OF ROAD IN ACCORDANCE W/ I.D.O.I. REQUIREMENTS.
- ADA HANDICAP RAMPS SHALL BE INSTALLED AT ALL LOCATIONS DELINEATED ON PLANS AS WELL AS AT ALL LOCATIONS WHERE SIDEWALK ADJUTS DRIVES OR ROADWAYS.
- CONTRACTOR SHALL COMPARE ARCHITECTURAL AND ENGINEERING PLANS FOR INTERFACE COMPATIBILITY.
- ALL CURB AND GUTTER SHALL BE 6:12 UNLESS OTHERWISE NOTED ON PLANS.
- PAVEMENT STRIPING TO BE TRAFFIC YELLOW TWO COATS UNLESS OTHERWISE SPECIFIED ON PLANS.
- ALL TRAFFIC SIGNS SHALL BE INSTALLED AT 7' HIGH MEASURED FROM THE GROUND ELEVATION TO THE BOTTOM ON THE SIGN.

ZONING: B-3
 VILLAGE OF BARTLETT

ZONING: B-3
 VILLAGE OF BARTLETT

LEGEND

- PROPOSED CURB & GUTTER
- EXISTING CURB & GUTTER
- PROPERTY LINE
- SETBACK LINE
- RETAINING WALL
- 8' HIGH VINYL STONE FENCE
- 6' HIGH SOLID VINYL PRIVACY FENCE
- EXISTING 4' HIGH CHAIN LINK FENCE
- CONCRETE
- LIGHTS
- PUMPS

REVISIONS

NO.	DATE	PER VILLAGE COMMENTS	DESCRIPTION
1	11/12/18		

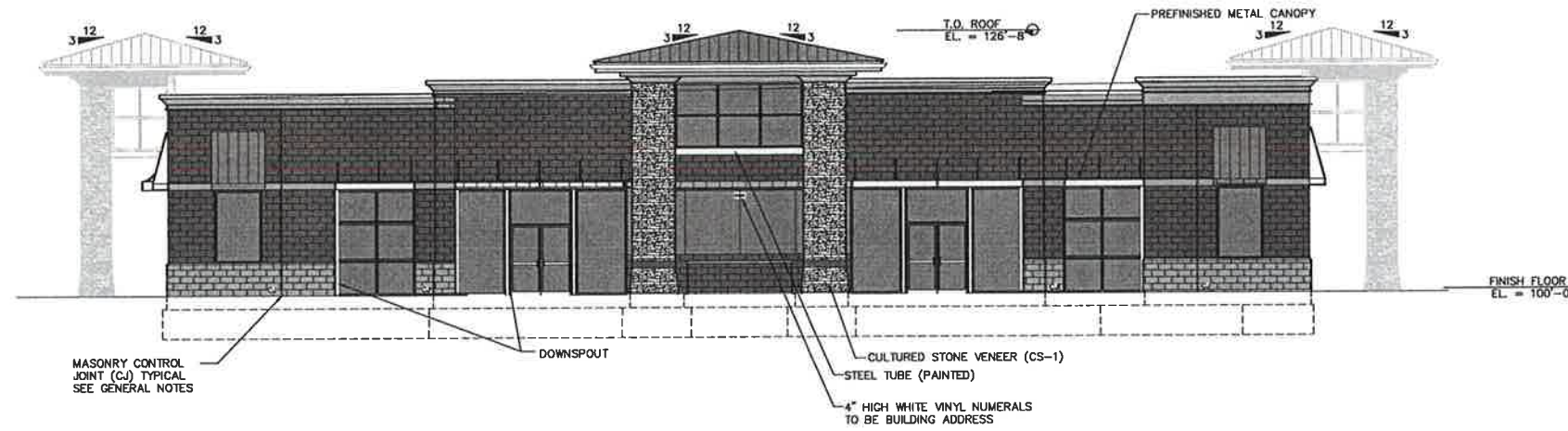
SITE PLAN

BUCKY'S
 IL. ROUTE 59 & NORWOOD LANE
 BARTLETT, ILLINOIS

Craig R. Knoche & Associates
 Civil Engineers, P.C.
 1161 Commerce Drive • Geneva, IL 60134 • phone (630) 845-1270 • fax (630) 845-1275

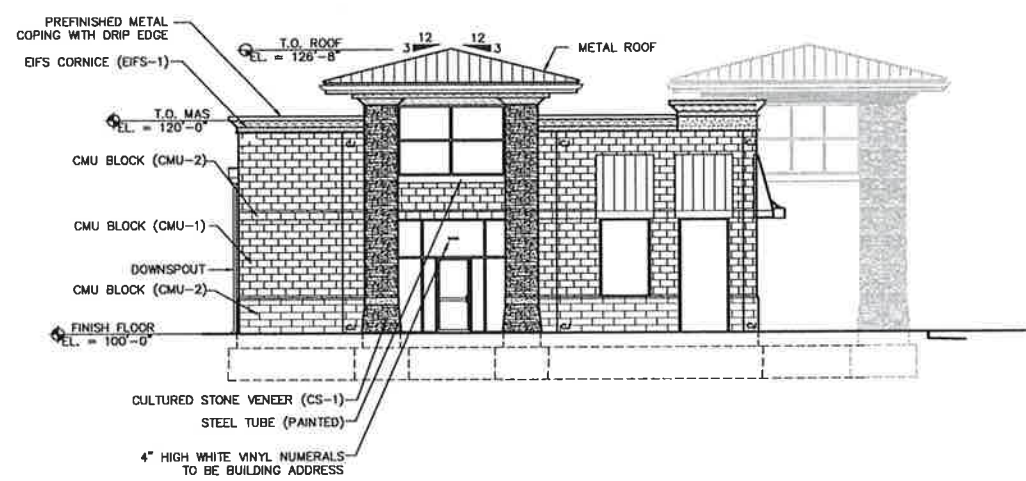
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 JOB NO: 931.02
C1.1
 SHEET NO.

BARTLETT, ILLINOIS



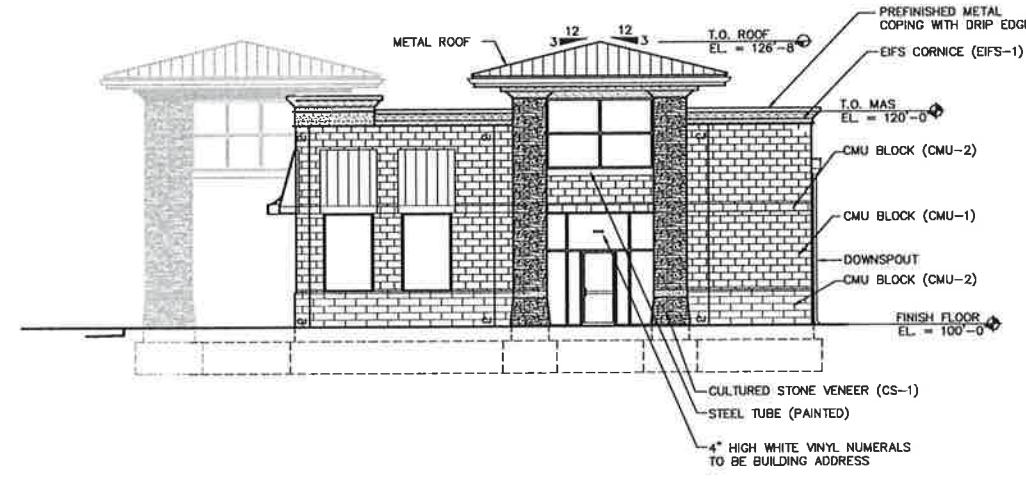
FRONT BUILDING ELEVATION

SCALE: 1/8" = 1'-0"



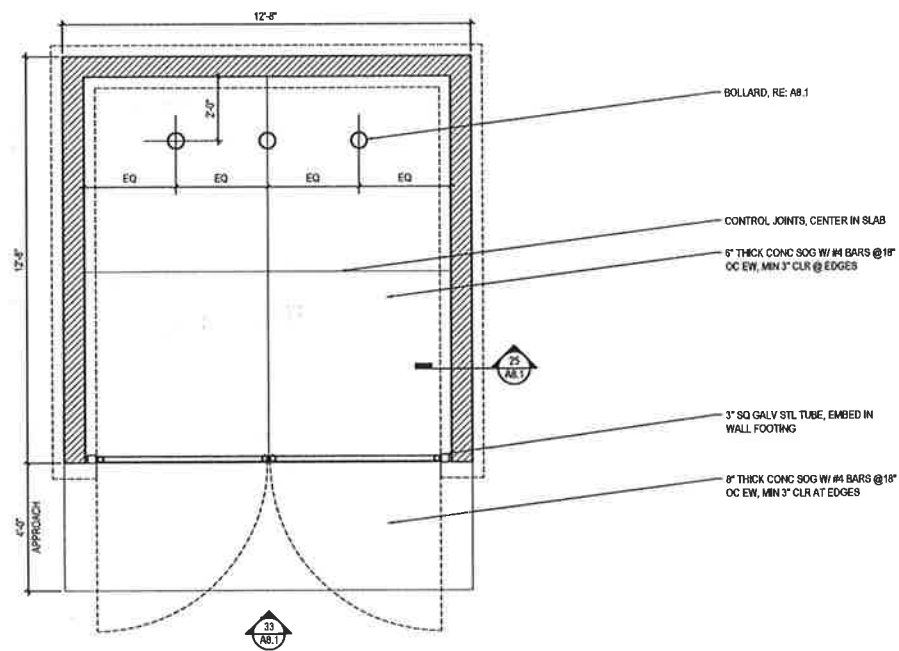
SIDE BUILDING ELEVATION

SCALE: 1/8" = 1'-0"



SIDE BUILDING ELEVATION

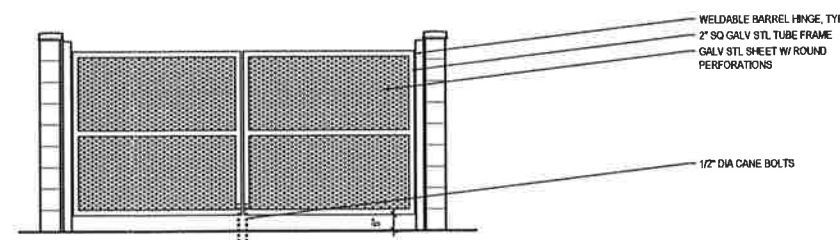
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DUMPSTER SCREEN PLAN

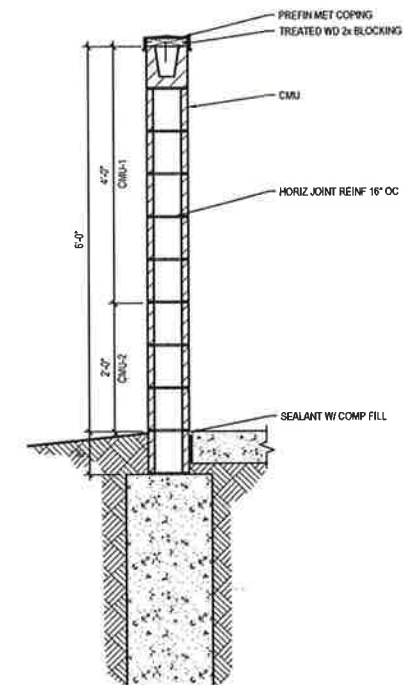
SCALE: 3/8" = 1'-0"

MATERIAL & COLOR LEGEND		
MARK	MATERIAL	STYLE / SIZE / COLOR
-	STANDING SEAM METAL	COPPER COAT
CMU-1	CONCRETE MASONRY, 8x8x16	TERRA COTTA
CMU-2	CONCRETE MASONRY, 8x8x16	PURTY



DUMPSTER SCREEN ELEVATION

SCALE: 3/8" = 1'-0"



SECTION

SCALE: 3/4" = 1'-0"

PROGRESS
PRINT
(NOT FOR
CONSTRUCTION)

BUCKY'S #931.02

BARTLETT, ILLINOIS

ROBERT W. ENGEL AND ASSOCIATES, ARCHITECTS

2443 South 156th Circle

Omaha, NE 68130-2503

(402) 330-8287 Fax: (402) 330-8331

email: RWEArchitects@RWEArchitects.com



Issue Date: January 10, 2018

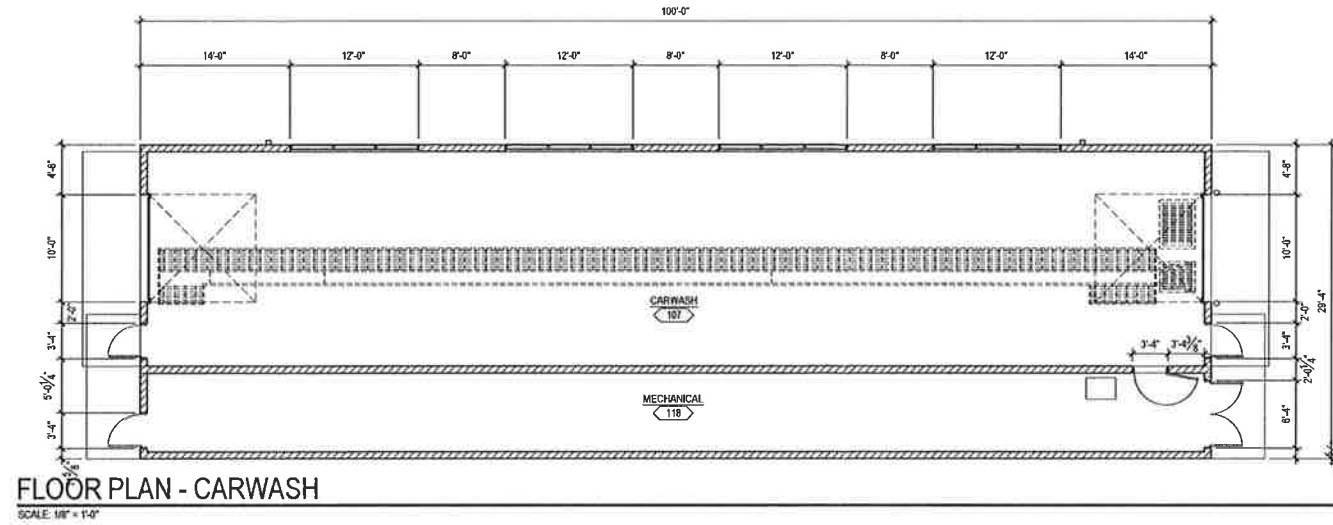
Project Number: GS

Drawn by: RWE

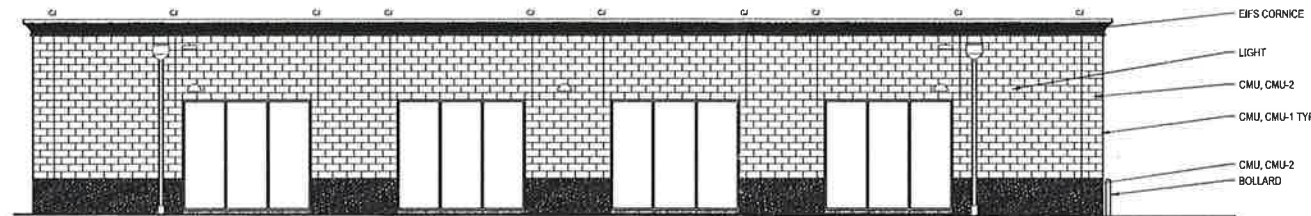
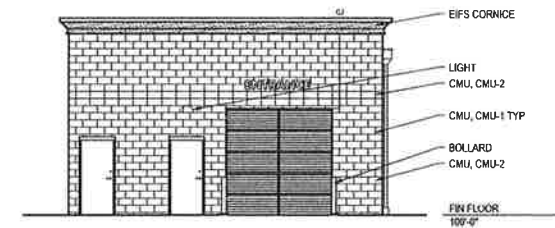
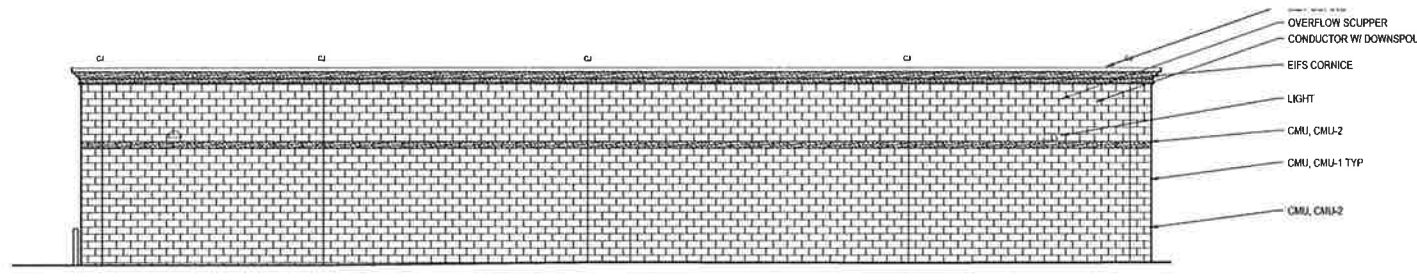
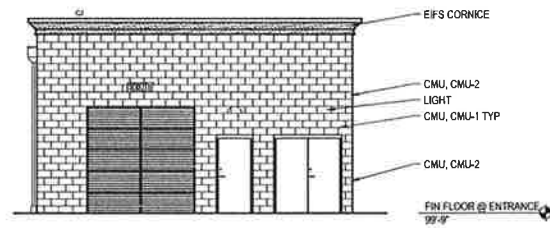
Checked by:

Sheet Name:

Revisions to the Set	Date
No.	Date



MATERIAL & COLOR LEGEND		
MARK	MATERIAL	STYLE / SIZE / COLOR
-	STANDING SEAM METAL	COPPER COAT
CMU-1	CONCRETE MASONRY	TERRA COTTA
CMU-2	CONCRETE MASONRY	PUTTY



PROGRESS
PRINT
(NOT FOR
CONSTRUCTION)

BUCKY'S #931.02
BARTLETT, ILLINOIS

RWE
ROBERT W. ENGEL AND ASSOCIATES, ARCHITECTS
2443 South 156th Circle
Omaha, NE 68130-2503
(402) 330-8287 Fax: (402) 330-8331
email: RWEArchitects@RWEArchitects.com

Project Number: — GS		Issue Date: January 10, 2018		Revisions to the Set	
Checked by:	Drawn by:	No.	Date	No.	Date
RWE	RWE				
Sheet Name: CARWASH PLANS & ELEVATIONS					

A1.7

Bucky's Convenience Store

11. Route 59 and Norwood Lane
Bartlett, Illinois

DAVID R. McCALLUM ASSOCIATES, INC.
LANDSCAPE ARCHITECTS
380 N. Milwaukee Avenue | Libertyville, Illinois 60048
T 847.322.8275 | F 847.322.0104



Landscape Plan

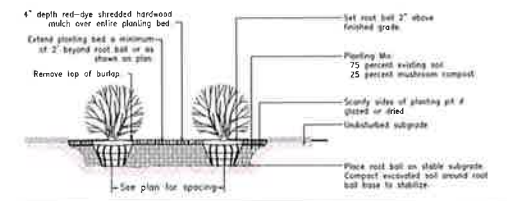
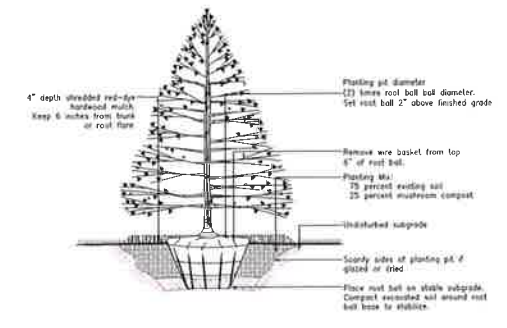
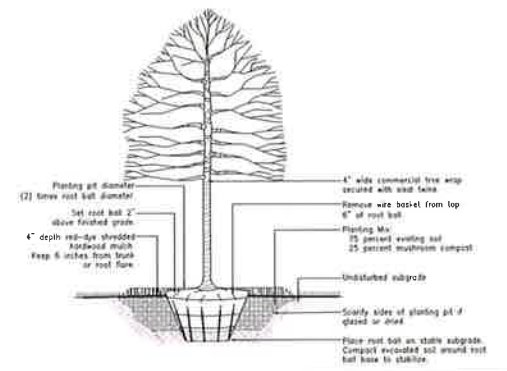
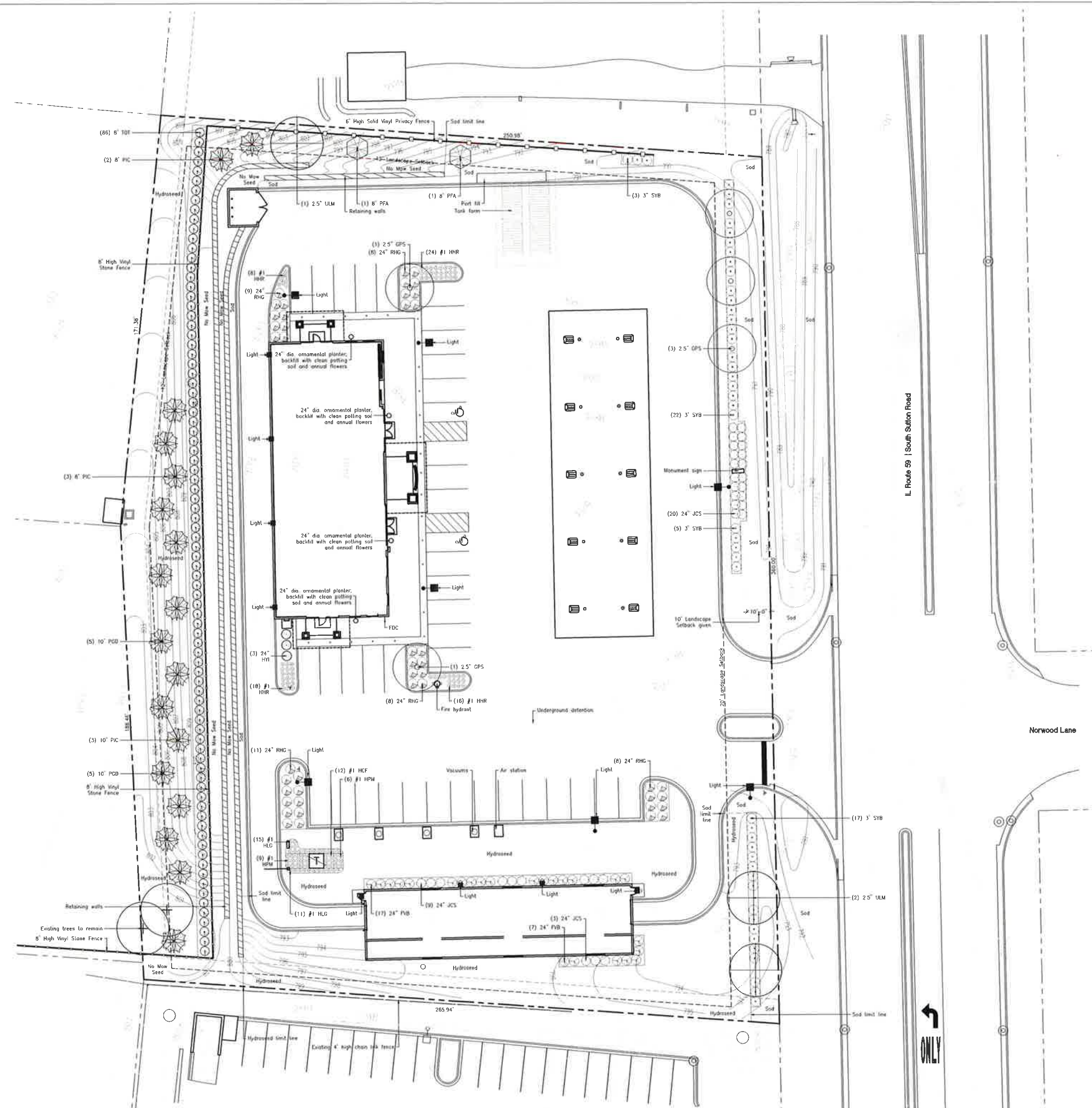
Mark	Description	Date
13	Per Village Comments	11.03.18
12	For Review	11.13.18
11	For Review	10.08.18
10	Site Plan Modification	09.26.18
9	Per Owner Comments	06.01.18
9	Per Village Comments	05.18.18
8	Per Village Comments	05.18.18
7	Site Plan Modification	04.25.18
6	For Review	03.26.18
5	For Review	02.08.18
4	For Review	02.07.18
3	For Review	01.10.18
2	For Review	01.05.18
1	For Review	01.04.18

Number
47467

Scale
1" = 20'

File
47467P17A

Sheet
L10



Plant List

Shade Trees	Key	Qty	Size	Botanical Name	Common Name	Remarks
GPS	5	2.5"	Ginkgo biloba 'Princeton Sentry'	Princeton Sentry Ginkgo	BB	
ULM	3	2.5"	Ulmus 'New Horizon'	New Horizon Elm	BB	

Evergreen Trees	Key	Qty	Size	Botanical Name	Common Name	Remarks
PFA	2	8'	Picea pungens 'Fat Albert'	Fat Albert Blue Spruce	BB	
FGD	10	10'	Picea canadensis 'Densata'	Black Hills Spruce	BB	
PIC	5	8'	Picea pungens	Colorado Green Spruce	BB	
PIC	3	10'	Picea pungens	Colorado Green Spruce	BB	

Shrubs	Key	Qty	Size	Botanical Name	Common Name	Remarks
FVB	24	24"	Forsythia viridissima 'Bronzeville'	Bronze Dwarf Forsythia	BB	
HRT	3	24"	Hydrangea arborescens 'Incrediball'	Incrediball Smooth Hydrangea	BB	
JCS	32	24"	Jungfernstrich 'Sargent'	Sargent Juniper	BB	
RHG	44	24"	Rhus aromatica 'Crisp-Leaf'	Crisp-Leaf Fragrant Sumac	BB	
SYB	47	3'	Syringa 'Blauer Zaun'	Blooming Blue Lilac	BB	
TOT	85	8"	Thuja occidentalis 'Topsylva'	Weymouth Arborvitae	BB	

Perennials	Key	Qty	Size	Botanical Name	Common Name	Remarks
HCF	12	#1	Hamamelis 'Chicago Fire'	Chicago Fire Daylily	Container	
HRT	56	#1	Hamamelis 'Happy Returns'	Happy Returns Daylily	Container	
HLG	25	#1	Hamamelis 'Little Gopette'	Little Gopette Daylily	Container	
HPM	15	#1	Hamamelis 'Pardon Me'	Pardon Me Daylily	Container	

No Mow Seed Mix

Botanical Name	Common Name	Pounds per Acre
Festuca commutata	Longleaf II Chewings Fescue	50
Festuca obtusa	Nodding Fescue	50
Festuca ovina	Sheeps Fescue	50
Festuca rubra	Creeping Red Fescue	50

Cover with NAG S75EN Erosion Control Blanket (biodegradable)

Notes

Landscape contractor shall provide all plant material based on the size, genus, species and cultivar included in the Plant List. Any substitutions shall be approved by the Owner, local municipality and Landscape Architect.

Shredded hardwood mulch shall be red-dye.

Tree branching shall start no less than six feet (6') above the pavement measured from pavement level.

To insure visibility, shrubs shall not exceed a mature height of thirty inches (30") above pavement on landscape islands at the end of parking rows.

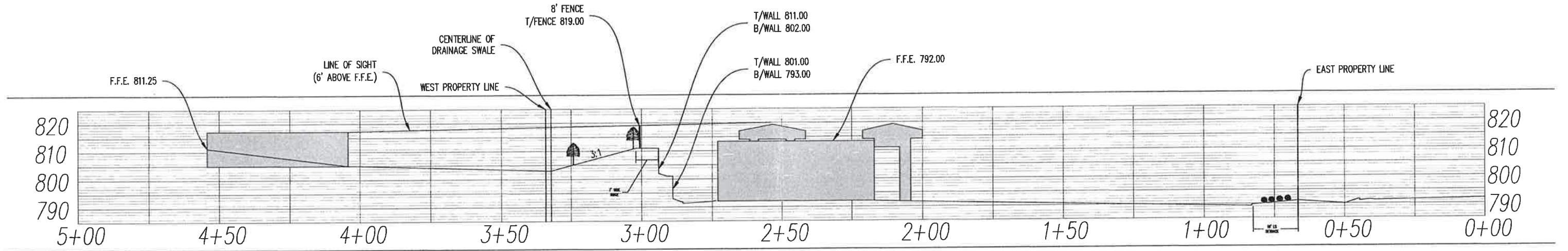
IL-59 & Norwood, Bartlett



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OCT 19 2018

VILLAGE OF
BARTLETT



Sound Evaluation for the Proposed Car Wash in Bartlett on Route 59

January 22, 2018

Acoustic Associates conducted an analysis to examine the impact of noise from the proposed car wash on Route 59 just west of Norwood Lane in Bartlett. To accomplish this, we used noise emissions data from a car wash currently operated by the petitioner in Vernon Hills. At that location, measurements were made at the exit, the entrance, and at different angles, in order to project accurate sound levels at various locations surrounding the new car wash. In addition, we inspected the Bartlett site to examine its topography and conducted ambient noise monitoring to document the current background noise levels that exist near the residents adjacent to the site.

Based on the blower noise emissions data and allowing for the reduction in noise due to directivity, distance, and the retention wall, we conclude that the noise from the car wash will meet the Illinois State noise code. Moreover, there would be no noise impact from the car wash because the projected sound levels are lower than the median ambient noise levels recorded at the site.

Ambient Noise Levels

To assess the **ambient noise** at this site, we set up professional grade audio equipment to record the noise from about 12:00 PM on November 29 to 11:00 AM on the next day. We located this equipment at the locations marked on the aerial photo in **Figure 1**. The upper location was at the northwest corner of the site.



Figure 1 - Aerial view showing the two locations used to locate the audio recording equipment to obtain ambient noise data.

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OCT 19 2018

VILLAGE OF
BARTLETT

It was near the residence on Lot 3. The lower location was at the southwest corner of the site. It was near the residence on Lot 1 3. The primary noise at these recording locations was vehicle traffic on Route 59, a primary, 4-lane highway. Secondary noise originated from traffic to the west on Sayer Road, a secondary, 2-lane road.

Ambient noise refers to the existing, all-encompassing sound at a given place in a community. It is a composite of sounds from many sources near and far.

The two recordings in the field were analyzed in our lab to determine the sound levels at 1-second intervals. These levels are shown in **Figure 2** for the north monitoring location and **Figure 3** for the south monitoring location. In both figures, the thin, blue line shows the variation in the **A-weighted sound level** at 1-second intervals. The high peaks in the curve were typically from motorcycles, trucks, and airplane flyovers.

The **A-weighted sound level** is the standard metric to measure sound and is expressed in decibels (dB). It mimics how we hear because it is a filter that diminishes the low frequencies.

The thick, red line is the median sound level, which reflects the trend in the ambient noise. The median levels are the highest in the late afternoon and early morning hours, specifically 63 dB at the south location and 67 dB at the north location. After the evening rush hour, the ambient noise tapers off - but does not drop significantly (i.e. by 5 dB) until after 10 PM.

Projected Noise Levels

Based on the data obtained at the Vernon Hills car wash, we expect a sound level of 83 dB 50 feet from the exit. The opening that faces the residents is the entrance and is 7 dB lower than the exit. Also, as you move to the side of the exit or the entrance, the sound level drops off - by as much as 11 dB at an angle of 90° from the opening. A drop of this amount means the sound is half as loud.

There are several residential lots to the west and the north of the car wash property. Two representative homes close to the carwash property are on Lots 1 and 3. Using the Vernon Hills data, we projected the noise levels to these properties based on the standard attenuation for sound over distance (i.e., 6 dB per doubling of distance) and the calculated effect of the retaining wall (which acts as a large noise barrier). The projected level is below the State of Illinois daytime limit of 55 dBA for noise radiated from commercial property (Class B) to residential property (Class A).

A potential noise impact could occur if the carwash noise levels exceed the median ambient noise for a sufficiently long duration. To visualize the relationship between the projected sound level and the ambient noise levels, we plotted the car wash blower noise level and the vacuum level on the ambient noise graphs of **Figure 2** and **Figure 3**. As seen in these charts, the car wash noise levels will be well below the median ambient noise level at the site. Therefore, it can be concluded that the operation of this facility will pose no impact during the operation hours of 6:00 AM to 10:00 PM.

Submitted by,



Thomas Thunder, AuD, FAAA, INCE Bd. Cert.
Principal Audiologist and Acoustical Specialist
Adjunct Faculty – Northern Illinois University, Rush University, Salus University

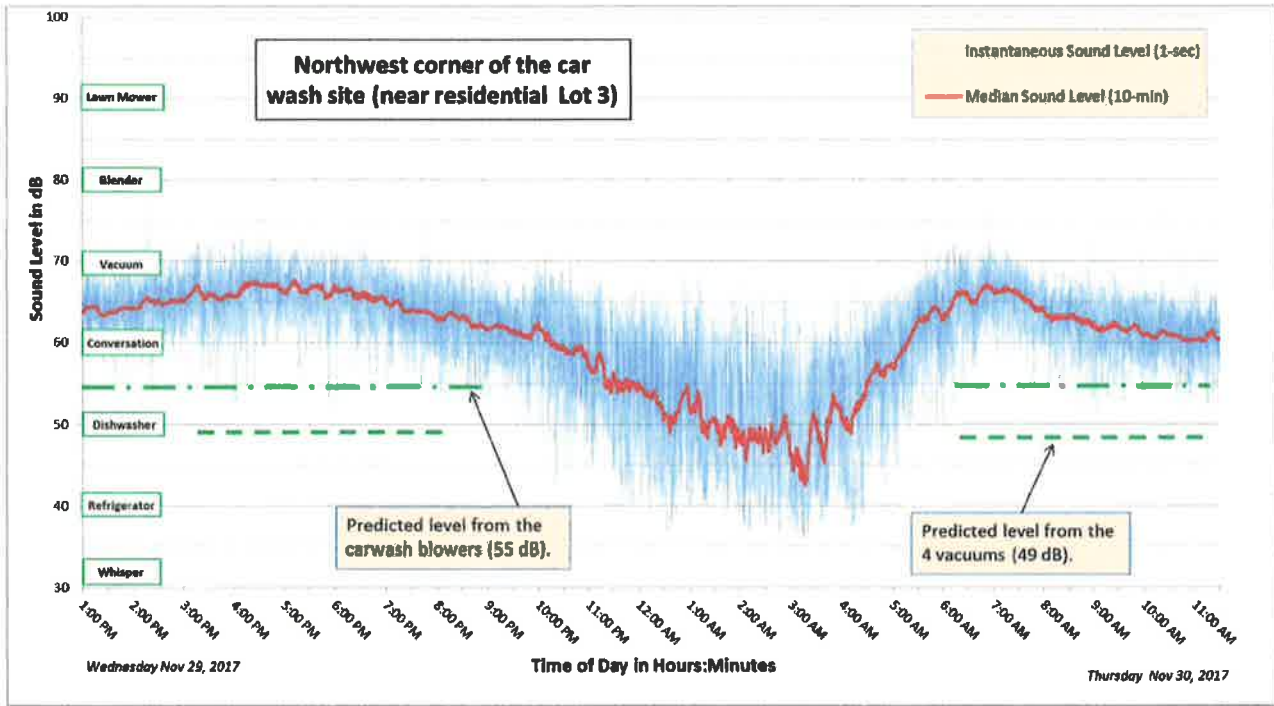


Figure 2 – Plot showing the expected car wash sound levels at the residential location on Lot 3 compared to the ambient noise level measured near this location (i.e., at the northwest corner of the car wash property).

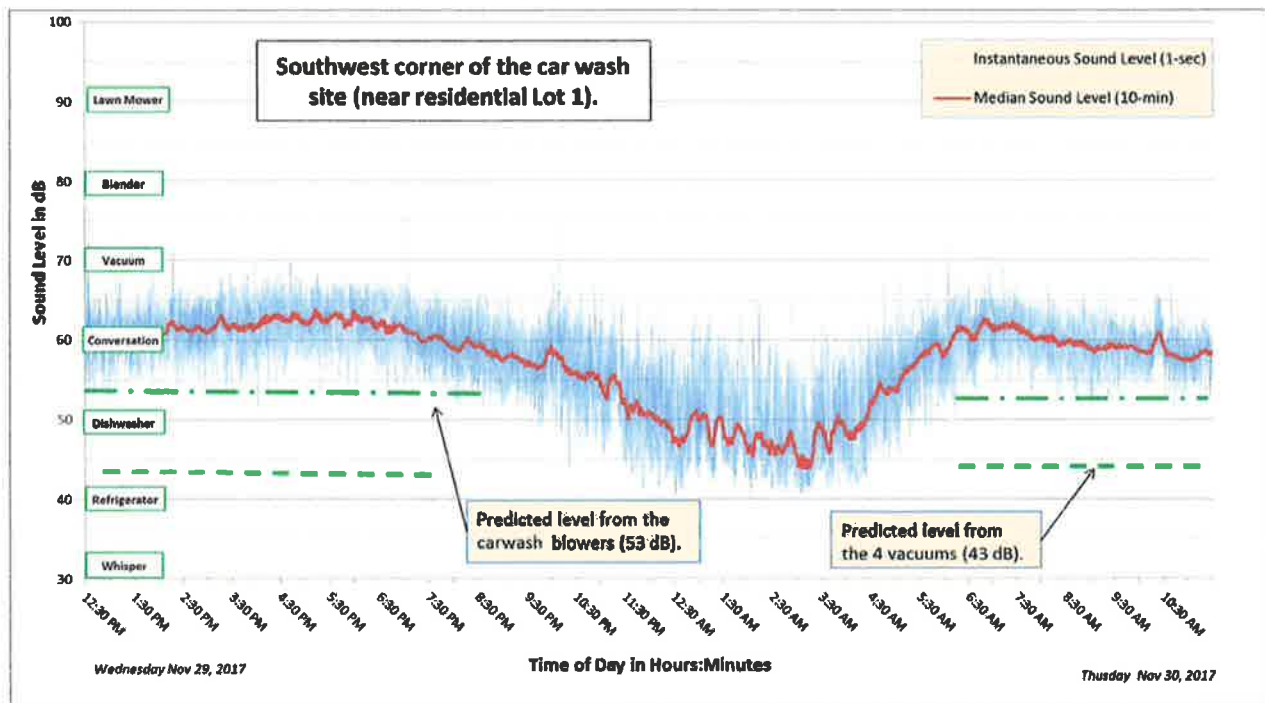


Figure 3 - Plot showing the expected car wash sound levels at the residential location on Lot 1 compared to the ambient noise levels measured near this lot (i.e., at the southwest corner of the car wash property).



2351 South Dirksen Parkway ▪ Springfield, IL 62703
Phone: 217.670.0563 ▪ Fax: 217.245.5398
www.quiggengineering.com

QUIGG ENGINEERING INC

Memorandum

To: Richard McMahon
Buchanan Energy

From: Michael Vail, P.E.
Operations Manager
Quigg Engineering Inc.

Date: July 25, 2018

Subject: Bucky's Gas Station
IL 59 and Norwood Lane
Bartlett, Illinois

This memorandum summarizes a traffic study conducted for a proposed Bucky's Gas Station with a convenience store and car wash located on the west side of IL 59 (Sutton Road) at Norwood Lane. The purpose of the study was to observe existing traffic patterns in the area of the proposed development site, estimate the traffic generated by the proposed development, and determine if a traffic signal would be warranted for the intersection at the proposed development entrance on IL 59.

Existing Conditions

The proposed development site is located in Bartlett, IL on the west side of IL 59 (Sutton Road) at Norwood Lane and will have 10 fuel dispensers, a 7,144 square foot convenience store, and a car wash. Proposed access to the development will be provided through one access drive on IL 59 at the intersection with Norwood Lane. The existing intersection of IL 59 at Norwood Lane is a three-leg intersection, and the proposed development would convert the intersection to a four-leg intersection. The surrounding land use is primarily commercial, with office, retail, and restaurant uses to the south and east of the site. There are some residential areas to the north and east of the proposed site as well. West of the site, the land use is primarily residential developments.

Illinois 59 is a north-south roadway classified as an Other Principal Arterial and is also designated as a Strategic Regional Arterial (SRA) route. At the unsignalized intersection with Norwood Lane, IL 59 has two through lanes in each direction and a southbound left turn lane. The posted speed limit on IL 59 is 45 mph. Norwood Lane is an east-west roadway classified as a Local Road and includes one through lane in each direction with stop-controlled left and right turn lanes at the intersection with IL 59. The posted speed limit on Norwood Lane is 25 mph.

Traffic counts were performed for a 24-hour period on Wednesday, June 13, 2018 at the intersection of IL 59 and Norwood Lane. These counts indicated the AM peak hour occurred between 7:00 a.m. and 8:00 a.m. and the PM peak hour occurred between 4:45 p.m. and 5:45 p.m. The existing peak hour traffic volumes are shown in **Figure 1**. See **Appendix** for full 24-hour traffic count volumes.

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BARTLETT

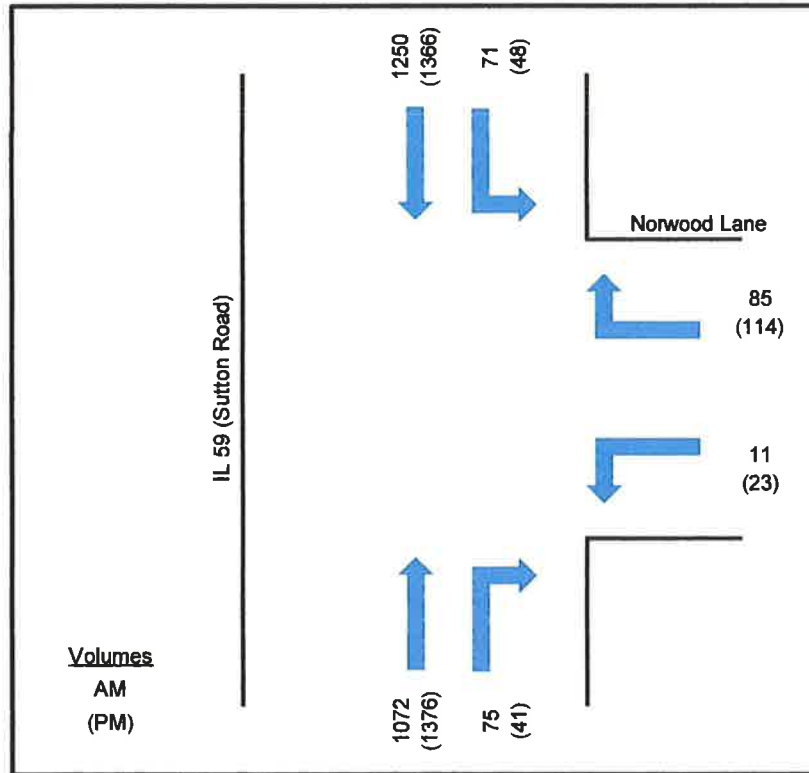


Figure 1: Peak Hour – Existing (2018) Traffic Volumes

Traffic Generation Summary

Trip Generation

The additional traffic to be generated by the proposed development was estimated using the Institute of Transportation Engineers' (ITE) Trip Generation Manual, 10th Edition Land Use: 960 – Super Convenience Market/Gas Station. Based on 20 fueling positions (10 fuel dispensers) and using the charts for the AM and PM Peak Hours of Generator (see **Appendix**), it was determined the site would generate 426 trips in the AM peak hour and 405 trips in the PM peak hour. It was assumed that, based on the ITE Trip Generation Manual, the directional distribution for the site would be 50% entering and 50% exiting traffic. **Table 1** shows the total site-generated traffic for the AM and PM peak hours.

Table 1: Site-Generated Traffic

Peak Hour (DHV)	TOTAL	Average Rate	# Fueling Positions	Directional Distributional	Entering	Exiting
AM	426	21.30	20	50%	213	213
PM	405	20.25	20	50%	204	204

Trip Distribution

The trip distribution for the proposed gas station was derived using Tables E.37 and E.38 in the ITE Trip Generation Handbook, 3rd Edition (see **Appendix**). The tables show average pass-by trip percentages of 62% and 56% for the AM and PM peak periods, respectively for Land Use Code 945 – Gasoline/Service Station with Convenience Market. The directional distribution of trips entering the proposed development

was derived from the distribution of the northbound and southbound traffic on IL 59 obtained from the 24-hour traffic counts. For the direct trips (38% of AM, 44% of PM site-generated trips), the exiting trips were assumed to return to the originated location while the pass-by trips were assumed to continue along the original path. Based on the traffic counts, in the AM peak hour, SB traffic accounted for 51.5%, NB traffic accounted for 44.7%, and WB traffic accounted for 3.7% of total traffic volumes at the intersection. In the PM peak hour, SB traffic accounted for 47.6%, NB traffic accounted for 47.7% and WB traffic accounted for 4.6% of total traffic volumes at the intersection. **Table 2** shows the directional distribution of the direct trip volumes and **Figure 2** shows the peak hour volume adjustment based on the direct trip volumes. **Table 3** shows the directional distribution of the pass-by trip volumes and **Figure 3** shows the peak hour volume adjustment based on the pass-by trip volumes. **Figure 4** shows the total peak hour volume adjustment based on the site-generated direct and pass-by trips for the proposed development.

Table 2: Direct Trip Volume Directional Distribution

Trip Generation - Direct Trip Volume Adjustment (38% AM, 44% PM)				
		IL 59 (Sutton Road)		Norwood Lane
Peak Hour (DHV)	Entering Trips	RT (SB) - (51.5%)	LT (NB) - (44.7%)	TH (WB) - (3.7%)
AM	81	42	36	3
	Exiting Trips	LT (EB) - (51.5%)	RT (EB) - (44.7%)	TH (EB) - (3.7%)
AM	81	42	36	3
	Entering Trips	RT (SB) - (47.6%)	LT (NB) - (47.7%)	TH (WB) - (4.6%)
PM	90	43	43	4
	Exiting Trips	LT (EB) - (47.6%)	RT (EB) - (47.7%)	TH (EB) - (4.6%)
PM	90	43	43	4

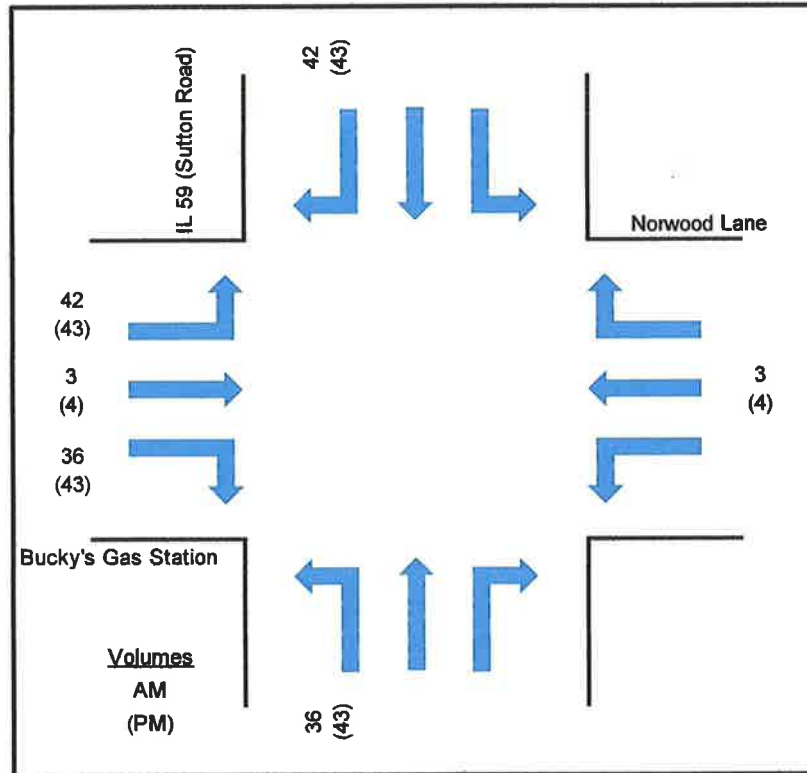


Figure 2: Peak Hour Site-Generated Direct Trip Volume Adjustment

Table 3: Pass-By Trip Volume Directional Distribution

Trip Generation - Pass-By Trip Volume Adjustment (62% AM, 56% PM)							
Peak Hour (DHV)	Entering Trips	IL 59 (Sutton Road)			Norwood Lane		
		RT (SB) - (51.5%)	LT (NB) - (44.7%)		TH (WB) - (3.7%)		
AM	132	68		59		5	
	Exiting Trips	RT (EB) - (48.8%)	TH (EB) - (5.7%)	LT (EB) - (41.8%)		RT (EB) - (0.4%)	LT (EB) - (3.3%)
AM	132	64	8	55		1	4
	Entering Trips	RT (SB) - (47.6%)		LT (NB) - (47.7%)		TH (WB) - (4.6%)	
PM	114	54		54		6	
	Exiting Trips	RT (EB) - (46.0%)	TH (EB) - (3.0%)	LT (EB) - (46.4%)		RT (EB) - (0.8%)	LT (EB) - (3.8%)
PM	114	52	3	53		1	5

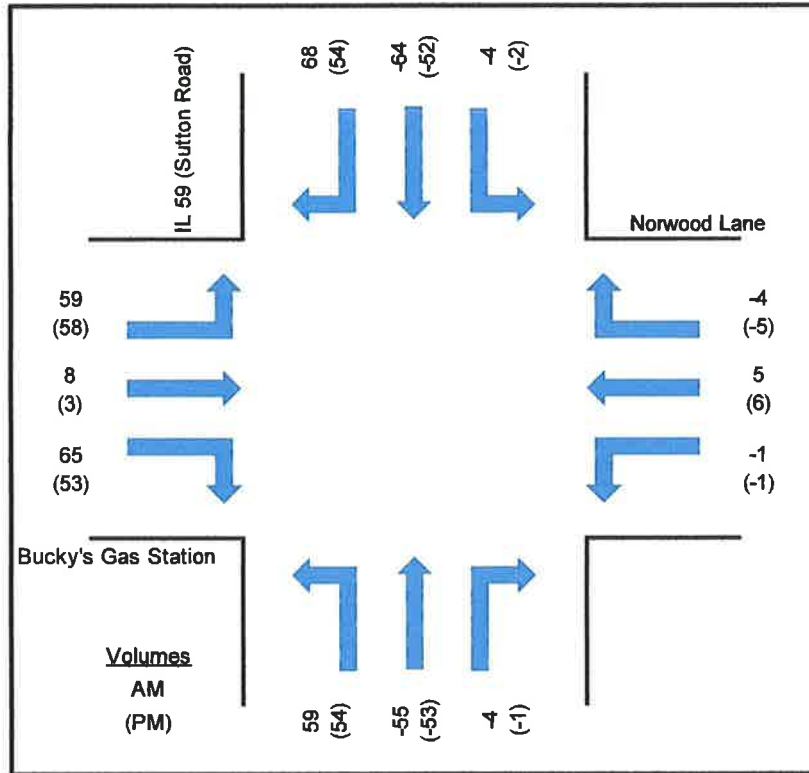


Figure 3: Peak Hour Site-Generated Pass-By Trip Volume Adjustment

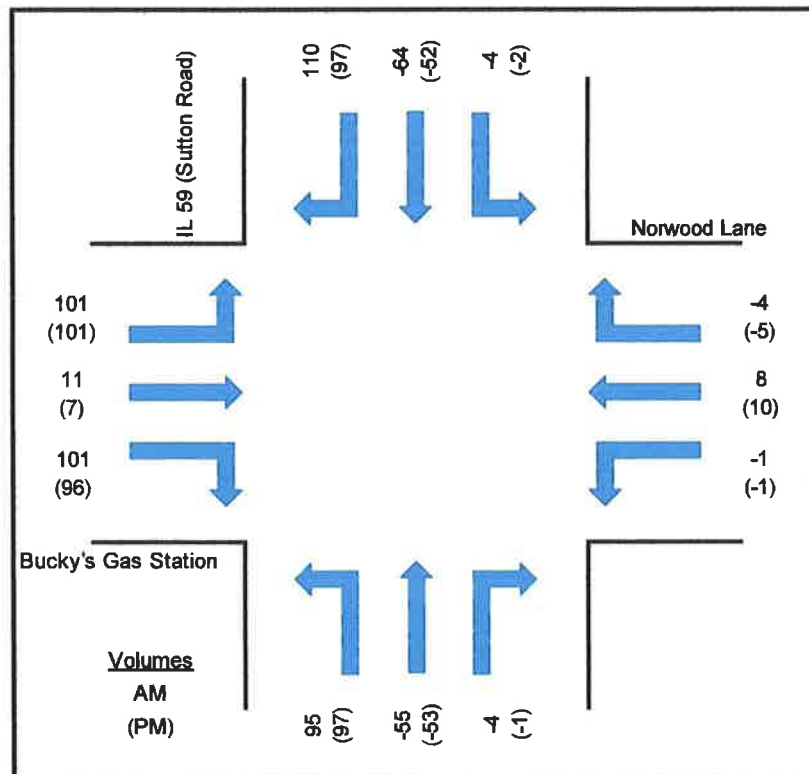


Figure 4: Peak Hour Total Site-Generated Traffic Adjustment

Projected Traffic Volumes

The total traffic volumes for the intersection are a combination of the existing traffic volumes with projected growth to the construction year and the proposed development generated traffic volumes. Construction of the gas station is proposed for 2019. Data provided by the Chicago Metropolitan Agency for Planning (CMAP) shows a growth rate for IL 59 at Norwood Lane of 0.6% per year, on average. A copy of the CMAP letter is included in the **Appendix**. The growth rate was applied to the existing (2018) traffic volumes at the intersection of IL 59 and Norwood Lane, shown in Figure 1, to obtain the base construction year (2019) volumes without the proposed development. These volumes were then combined with the site-generated traffic volumes, shown in Figure 4, to obtain the total traffic volumes for the construction year (2019). The total traffic volume distribution is shown in **Figure 5**.

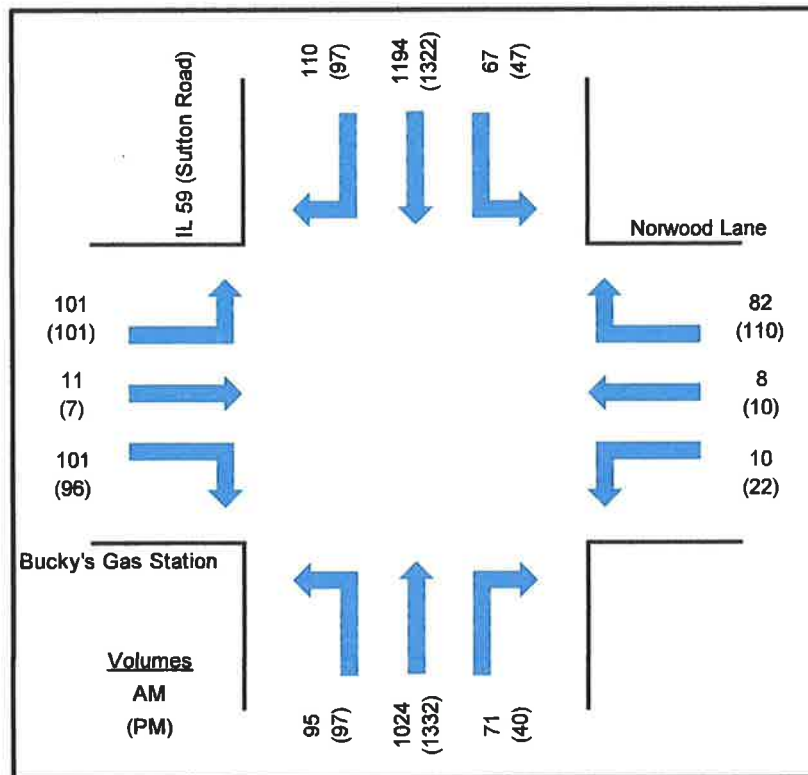


Figure 5: Peak Hour Construction Year (2019) Traffic with Site-Generated Traffic Adjustment

Signal Warrant Analysis

A traffic signal warrant study was performed using the proposed construction year (2019) traffic volumes at the intersection of IL 59 and Norwood Lane to determine if the intersection meets the minimum requirements to warrant the installation of a traffic control signal. The investigation of the need for a traffic control signal includes an analysis of factors related to the existing operation and safety at the intersection and the potential to improve these conditions, and the applicable factors contained in Warrants 1 through 9 as detailed in Chapter 4C of the Manual on Uniform Traffic Control Devices for Streets and Highways (MUTCD). The warrants are listed below:

- Warrant 1, Eight-Hour Vehicular Volume
- Warrant 2, Four-Hour Vehicular Volume
- Warrant 3, Peak Hour
- Warrant 4, Pedestrian Volume
- Warrant 5, School Crossing
- Warrant 6, Coordinated Signal System

- Warrant 7, Crash Experience
- Warrant 8, Roadway Network
- Warrant 9, Intersection Near a Grade Crossing

Because IL 59 is an SRA Route, the Illinois Department of Transportation (IDOT) does not allow Warrants 2 and 3 to be used for justification of a traffic signal. Additionally, Warrants 4, 5, and 9 were not considered for this study as they do not apply to this intersection due to the lack of presence of pedestrians, school crossings, and rail crossings which are required for each of the warrants, respectively. Warrant 8 was also not applicable for this study as Norwood Lane does not meet the requirements to be considered a major route. Therefore, Warrants 1, 6, and 7 were further considered for this intersection.

Warrant 1 – Eight-Hour Vehicular Volume

Warrant 1 has two conditions which can be applied to an intersection. Condition A is intended for application at locations where a large volume of intersecting traffic is the principal reason to consider installing a traffic signal. Condition B is intended for application at locations where Condition A is not satisfied and where the traffic volume on a major street is so heavy that traffic on a minor intersecting street suffers excessive delay or conflict in entering or crossing the major street. The need for a traffic control signal shall be considered if a study finds that one of the following conditions exist for each of any 8 hours of an average day:

- A. The vehicles per hour given in both of the 100 percent columns of Condition A in Table 4C-1 (MUTCD) exist on the major-street and the higher-volume minor-street approaches, respectively, to the intersection; or
- B. The vehicles per hour given in both of the 100 percent columns of Condition B in Table 4C-1 exist on the major-street and the higher-volume minor-street approaches, respectively, to the intersection.

In applying each condition, the major-street and minor-street volumes shall be for the same 8 hours. On the minor street, the higher volume shall not be required to be on the same approach during each of these 8 hours.

According to Section 57-4.04(c) of the IDOT Bureau of Design and Environment (BDE) Manual, for a proposed development, “the 8-hour vehicle volume may be considered as 55% of the projected 30th maximum hour volume.” The 30th maximum hour volume is typically considered the design hourly volume (DHV), or the peak hour volume for the intersection based on the traffic count volumes. Table 4 shows the 8-hour maximum vehicle volumes based on the AM and PM peak hour volumes. These volumes represent the 8th highest hour of traffic during the day based on the DHV, meaning there are 7 other hours throughout the day with higher traffic volumes than the 8th maximum hourly volume. The 8th max hourly volume for the higher volume approach of the minor road (Proposed Development) is 117 vph based on the AM peak hour volumes at the proposed development and the 8th max hourly two-way volume for the major road (IL 59) is 1612 vph based on the PM peak hour volumes.

Table 4: 8-Hour Vehicle Volumes

Peak Hour - Traffic Volumes (DHV)	AM Peak		PM Peak	
	2-Way Volume	1-Approach	2-Way Volume	1-Approach
	Major Road (IL 59)	Minor Road (Proposed Development)	Major Road (IL 59)	Minor Road (Proposed Development)
Construction Year - 2019 Volumes	2561	213	2935	204
8th MAX Hourly - 0.55 x (DHV)	1409	117	1614	112

Warrant 1A cannot be met with the volumes of traffic for the proposed development. The two-way volumes on the major road (IL 59) meet the minimum volume of 600 vph for the 100 percent column for the major road in Table 4C-1 (see Appendix) for 8 hours, but the higher-volume minor-street approach (proposed development) does not meet the minimum volume of 150 vph. Condition B can be met with the volumes of traffic for the proposed development. The two-way volumes on the major road (IL 59) meet the minimum volume of 900 vph for the 100 percent column of Table 4C-1 and the higher-volume minor-street approach (proposed development) meets the minimum volume of 100 vph (per D1 SRA Route Signal Warrant Restrictions). Since only one of the two conditions needs to be met for the warrant to be satisfied, **Warrant 1 is satisfied.**

Warrant 6 – Coordinated Signal System

Warrant 6 states that a traffic control signal should be considered if a study finds that one of the following criteria is met:

- A. On a one-way street or a street that has traffic predominantly in one direction, the adjacent traffic control signals are so far apart that they do not provide the necessary degree of vehicular platooning.
- B. On a two-way street, adjacent traffic control signals do not provide the necessary degree of platooning and the proposed and adjacent traffic control signals will collectively provide a progressive operation.

Additionally, Warrant 6 should not be applied where the resultant spacing of traffic control signals would be less than 1,000 feet.

The nearest traffic signal to the north of Norwood Lane on IL 59 is approximately 6,000 feet, or 1.14 miles away at the intersection of IL 59 and Bartlett Road. This traffic signal is part of a coordinated traffic signal system that extends north along IL 59 from Bartlett Road to Golf Road.

To the south, the nearest traffic signal is at the intersection of IL 59 at Stearns Road approximately 1,150 feet, or 0.22 miles, away. This traffic signal is part of a different coordinated traffic signal system than the signal at Bartlett Road that extends south to Smith Road. The traffic signals at Bartlett Road and Stearns Road are not coordinated with each other. Illinois 59 and Stearns Road are both designated SRA routes. At the intersection of two major routes, the traffic signal timings are typically split equally between the two routes, which impacts green time available for traffic progression on each route. Traffic traveling southbound on IL 59 forms a platoon at the intersection with Bartlett Road. As the platoon travels further south, the vehicles begin to spread out more over the 7,150 feet between Bartlett Road and Stearns Road. By the time vehicles reach the intersection at Stearns Road, the platoon has dispersed and the likelihood of the platoon making it through the limited green time at Stearns Road decreases. A traffic signal at Norwood Lane would help manage the platoon and maximize the number of vehicles in the platoon clearing the intersection during the limited green time at Stearns Road.

While the warrant states the resultant spacing of traffic control signals should not be less than 1,000 feet, IDOT's BDE Manual states that for suburban SRA routes, "the spacing should not be less than ¼ mile (400 m) to an adjacent signal, but ½ mile (800 m) spacing is preferred." The BDE Manual does not have a restriction on the minimum spacing for traffic signals on an urban SRA Route. If a traffic signal were installed at the IL 59 and Norwood Lane intersection, the resultant traffic signal spacing would be approximately 6,000 feet from the signal at Bartlett Road and 1,150 feet from Stearns Road. Both of these distances satisfy the warrant requirement of a minimum 1,000 feet between traffic signals. This spacing would be similar to the spacing of 1,125 feet between the Stearns Road and Apple Valley Drive/Home Depot traffic signal located south of Stearns Road, and the area can be considered urban due to the numerous commercial and residential properties in the area. Therefore, installing a traffic signal at IL 59 and Norwood Lane would be beneficial to providing a progressive operation along IL 59 and **Warrant 6 is satisfied.**

Warrant 7 – Crash Experience

The conditions for Warrant 7 are intended for application where the severity and frequency of crashes are the principal reasons to consider installing a traffic signal. The need for a traffic control signal should be considered if a study finds that all of the following criteria are met:

- A. Adequate trial of alternatives with satisfactory observance and enforcement has failed to reduce the crash frequency; and
- B. Five or more reported crashes, of types susceptible to correction by a traffic control signal, have occurred within a 12-month period, each crash involving personal injury or property damage apparently exceeding the applicable requirements for a reportable crash; and
- C. For each of any 8 hours of an average day, the vehicles per hour (vph) given in both of the 80 percent columns of Condition A in Table 4C-1 (see Section 4C.02 in MUTCD), or the vph in both of the 80 percent columns of Condition B in Table 4C-1 exists on the major-street and the higher-volume minor-street approach, respectively, to the intersection, or the volume of pedestrian traffic is not less than 80 percent of the requirements specified in the Pedestrian Volume warrant. These major-street and minor-street volumes shall be for the same 8 hours. On the minor street, the higher volume shall not be required to be on the same approach during each of the 8 hours.

There have only been thirteen crashes at the intersection in the five years from 2011-2015 based on the most recent available crash data provided by IDOT. Of these thirteen crashes, only five crashes over the five year study period appear to be correctable with the installation of a traffic signal. Copies of the crash data are included in the **Appendix**. Since there were not enough crashes to meet the minimum criteria for this warrant, **Warrant 7 is not satisfied.**

Conclusions

The results of the study show that a traffic signal is warranted at the intersection of IL 59 and Norwood Lane. Based on the existing traffic and the proposed site-generated traffic, Warrant 1 – Eight-Hour Vehicular Volume, is satisfied. Additionally, Warrant 6 – Coordinated Signal System, is satisfied based on the benefits that a traffic signal at Norwood Lane would provide to better platoon traffic on IL 59 approaching Stearns Road. Warrant 7 – Crash Experience was not met due to the low number of correctable crashes at the intersection over a five year period (2011-2015). Warrants 2, 3, 4, 5, 8, and 9 were not applicable to the intersection.

Appendix

- Existing Traffic Counts
- ITE Trip Generation Manual Figures
- CMAP Letter
- MUTCD Table 4C-1
- Crash Data (2011-2015)
- Preliminary Site Plan

IL 59 at Norwood Lane

City: Bartlett
 County: DuPage
 District: 1

Dates: 6/13/2018 (AM)
 6/13/2018 (PM)

Summary of Traffic Survey

Route	Traffic From: N IL 59			Traffic From: S IL 59			TOTAL NORTH SOUTH	Traffic From: E Norwood Lane			TOTAL EAST WEST	GRAND TOTAL
	Going		TOTAL	Going		TOTAL		Going		TOTAL		
	E	S		N	E			S	N			
0:00	1	38	39	39	5	44	83	4	3	7	7	90
0:15	3	32	35	47	2	49	84	3	0	3	3	87
0:30	4	26	30	29	1	30	60	2	3	5	5	65
0:45	1	30	31	28	4	32	63	0	1	1	1	64
1:00	2	27	29	21	1	22	51	0	1	1	1	52
1:15	1	19	20	23	3	26	46	2	5	7	7	53
1:30	3	17	20	16	3	19	39	2	0	2	2	41
1:45	2	20	22	18	1	19	41	1	0	1	1	42
2:00	0	22	22	16	0	16	38	1	0	1	1	39
2:15	3	15	18	19	0	19	37	2	0	2	2	39
2:30	2	15	17	26	2	28	45	1	1	2	2	47
2:45	1	20	21	10	1	11	32	1	1	2	2	34
3:00	3	10	13	14	5	19	32	4	2	6	6	38
3:15	2	21	23	18	2	20	43	3	3	6	6	49
3:30	5	23	28	15	2	17	45	4	0	4	4	49
3:45	3	30	33	17	7	24	57	3	2	5	5	62
4:00	4	39	43	38	2	40	83	0	3	3	3	86
4:15	5	47	52	29	4	33	85	5	2	7	7	92
4:30	8	73	81	51	9	60	141	5	6	11	11	152
4:45	4	80	84	55	11	66	150	9	8	17	17	167
5:00	13	130	143	76	12	88	231	6	11	17	17	248
5:15	19	193	212	122	15	137	349	16	10	26	26	375
5:30	22	240	262	170	15	185	447	15	9	24	24	471
5:45	17	229	246	145	12	157	403	7	20	27	27	430
6:00	12	276	288	203	15	218	506	6	17	23	23	529
6:15	17	312	329	238	29	267	596	1	25	26	26	622
6:30	17	328	345	240	22	262	607	2	35	37	37	644
6:45	13	251	264	233	19	252	516	5	19	24	24	540
7:00	16	311	327	258	16	274	601	2	15	17	17	618
7:15	20	342	362	291	20	311	673	2	25	27	27	700
7:30	21	304	325	266	23	289	614	4	22	26	26	640
7:45	14	293	307	257	16	273	580	3	23	26	26	606
8:00	23	245	268	247	15	262	530	4	19	23	23	553
8:15	17	308	325	267	21	288	613	6	20	26	26	639
8:30	22	263	285	254	19	273	558	4	32	36	36	594
8:45	24	283	307	242	28	270	577	8	29	37	37	614
9:00	14	245	259	234	13	247	506	3	21	24	24	530
9:15	19	237	256	236	16	252	508	7	12	19	19	527
9:30	16	218	234	245	17	262	496	7	20	27	27	523
9:45	13	219	232	221	14	235	467	4	17	21	21	488
10:00	15	199	214	208	14	222	436	6	17	23	23	459
10:15	10	222	232	213	15	228	460	4	11	15	15	475
10:30	6	239	245	238	17	255	500	6	18	24	24	524
10:45	18	192	210	249	15	264	474	5	21	26	26	500
11:00	12	209	221	227	9	236	457	5	19	24	24	481
11:15	12	238	250	229	8	236	486	3	18	21	21	507
11:30	11	274	285	218	11	229	514	5	17	22	22	536
11:45	9	230	239	227	11	238	477	5	20	25	25	502

AM PEAK HOUR

IL 59 at Norwood Lane

City: Bartlett
 County: DuPage
 District: 1

Dates: 6/13/2018 (AM)
 6/13/2018 (PM)

Summary of Traffic Survey

Route	Traffic From: N IL 59			Traffic From: S IL 59			TOTAL NORTH SOUTH	Traffic From: E Norwood Lane			TOTAL EAST WEST	GRAND TOTAL
	Going		TOTAL	Going		TOTAL		Going		TOTAL		
	E	S		N	E			S	N			
12:00	25	260	285	231	29	260	545	8	20	28	28	573
12:15	18	244	262	290	15	305	567	4	18	22	22	589
12:30	12	231	243	270	17	287	530	6	24	30	30	560
12:45	9	236	245	238	7	245	490	6	17	23	23	513
13:00	12	237	249	236	13	249	498	6	16	22	22	520
13:15	22	238	260	213	4	217	477	1	25	26	26	503
13:30	10	199	209	251	15	266	475	7	20	27	27	502
13:45	14	258	272	270	17	287	559	6	12	18	18	577
14:00	11	221	232	231	6	237	469	7	21	28	28	497
14:15	17	220	237	271	14	285	522	7	23	30	30	552
14:30	9	263	272	283	12	295	567	2	17	19	19	586
14:45	13	271	284	318	18	336	620	1	23	24	24	644
15:00	11	288	299	303	10	313	612	6	20	26	26	638
15:15	12	343	355	272	11	283	638	3	18	21	21	659
15:30	16	281	297	300	10	310	607	4	21	25	25	632
15:45	20	326	346	353	13	366	712	3	20	23	23	736
16:00	7	349	356	366	7	373	729	2	23	25	25	754
16:15	14	328	342	324	8	332	674	4	13	17	17	691
16:30	20	327	347	305	10	315	622	1	21	22	22	654
16:45	11	329	340	298	10	308	649	8	28	37	37	686
17:00	8	372	380	351	11	362	742	6	23	29	29	771
17:15	11	321	332	324	7	331	663	3	23	26	26	689
17:30	18	344	362	402	13	415	777	5	40	45	45	822
17:45	17	318	335	309	10	319	654	7	18	25	25	679
18:00	12	325	337	363	13	376	713	2	18	20	20	733
18:15	10	304	314	289	10	299	613	3	18	21	21	634
18:30	8	279	287	264	9	273	560	2	11	13	13	573
18:45	7	263	270	292	7	299	589	2	9	11	11	600
19:00	7	212	219	230	10	240	459	0	9	9	9	468
19:15	9	277	286	236	9	245	531	2	12	14	14	545
19:30	12	171	183	252	3	255	438	4	13	17	17	455
19:45	11	228	239	244	7	251	490	4	15	19	19	509
20:00	6	220	226	242	9	251	477	4	9	13	13	490
20:15	7	183	190	205	12	217	407	1	12	13	13	420
20:30	15	200	215	227	8	235	450	2	10	12	12	462
20:45	9	174	183	195	5	200	383	5	7	12	12	395
21:00	9	213	222	207	6	213	435	3	7	10	10	445
21:15	7	169	176	179	9	188	364	5	10	15	15	379
21:30	10	158	168	169	5	174	342	2	6	8	8	350
21:45	3	165	168	130	7	137	305	1	11	12	12	317
22:00	2	115	117	110	7	117	234	4	7	11	11	245
22:15	6	90	96	109	9	118	214	2	9	11	11	225
22:30	3	93	96	117	8	125	221	5	7	12	12	233
22:45	4	92	96	82	8	90	188	3	6	9	9	195
23:00	5	81	86	86	7	93	179	0	7	7	7	186
23:15	4	59	63	80	3	83	146	5	6	11	11	157
23:30	3	46	49	69	5	74	123	2	3	5	5	128
23:45	2	49	51	62	6	68	119	3	3	6	6	125

PM PEAK HOUR

Super Convenience Market/Gas Station (960)

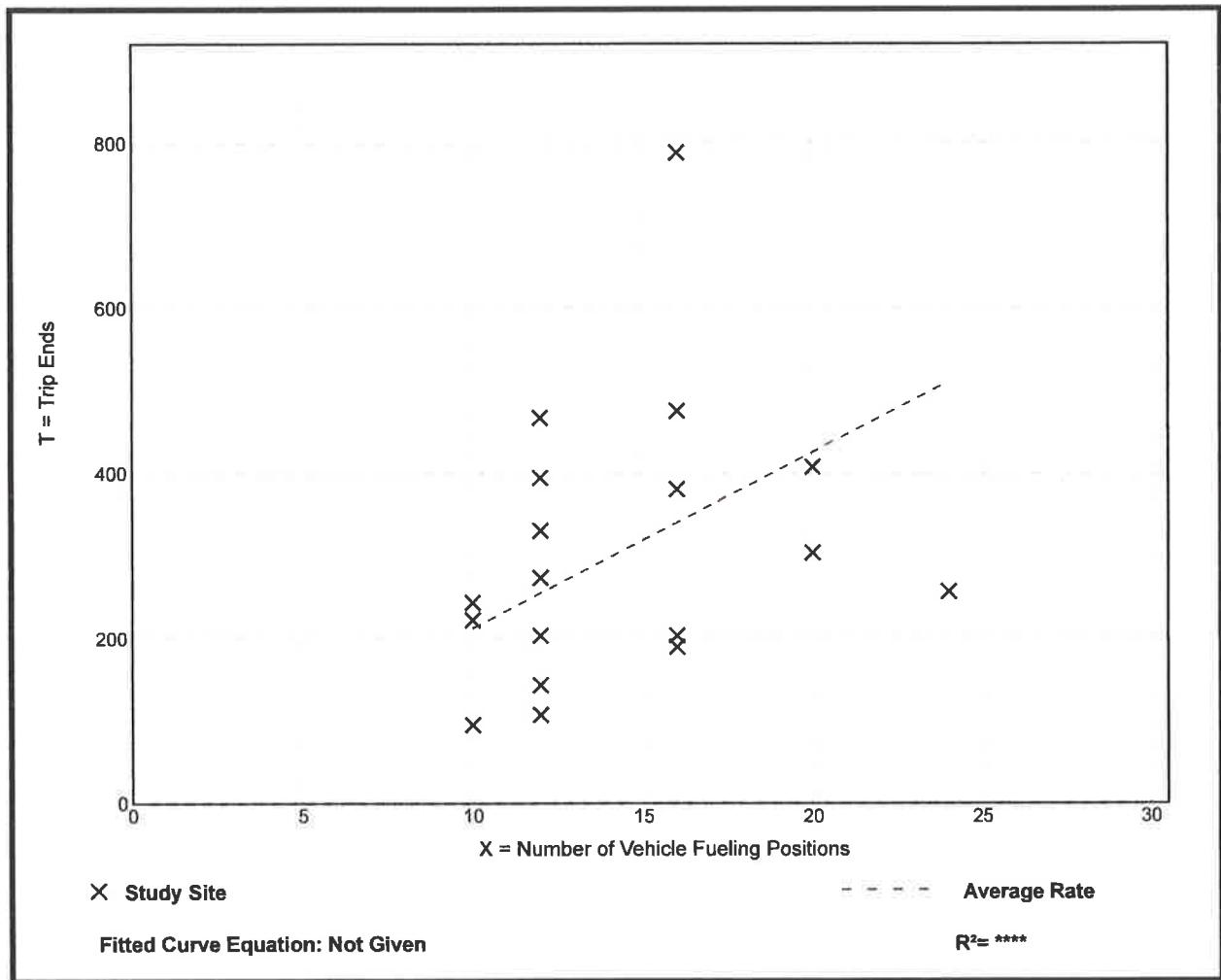
Vehicle Trip Ends vs: Vehicle Fueling Positions
On a: Weekday,
AM Peak Hour of Generator

Setting/Location: General Urban/Suburban
 Number of Studies: 18
 Avg. Num. of Vehicle Fueling Positions: 14
 Directional Distribution: 50% entering, 50% exiting

Vehicle Trip Generation per Vehicle Fueling Position

Average Rate	Range of Rates	Standard Deviation
21.30	9.00 - 49.31	11.15

Data Plot and Equation



**Table E.37 Pass-By and Non-Pass-By Trips Weekday, AM Peak Period
Land Use Code 945—Gasoline/Service Station with Convenience Market**

SIZE (1,000 SQ. FT. GFA)	VEHICLE FUELING POSITIONS	LOCATION	WEEKDAY SURVEY DATE	NO. OF INTERVIEWS	TIME PERIOD	PASS-BY TRIP (%)	NON-PASS-BY TRIPS (%)			ADJ. STREET PEAK HOUR VOLUME	SOURCE
							PRIMARY	DIVERTED	TOTAL		
0.8	8	Louisville area, KY	1993	61	7:00-9:00 a.m.	60	15	25	40	4,000	Barton- Aschman Assoc.
0.6	8	Louisville, KY	1993	48	7:00-9:00 a.m.	68	13	19	32	1,307	Barton- Aschman Assoc.
0.7	10	Louisville, KY	1993	47	7:00-9:00 a.m.	67	11	22	33	1,105	Barton- Aschman Assoc.
0.7	8	Louisville area, KY	1993	—	7:00-9:00 a.m.	56	22	22	44	1,211	Barton- Aschman Assoc.
0.7	10	Louisville area, KY	1993	—	7:00-9:00 a.m.	46	42	12	54	1,211	Barton- Aschman Assoc.
0.3	—	Louisville area, KY	1993	75	7:00-9:00 a.m.	72	15	13	28	—	Barton- Aschman Assoc.
0.8	8	Silver Spring, MD	1992	36	7:00-9:00 a.m.	47	14	39	53	3,095	RBA
0.4	8	Derwood, MD	1992	46	7:00-9:00 a.m.	75	0	25	25	3,770	RBA
2.2	8	Kensington, MD	1992	31	7:00-9:00 a.m.	47	34	19	53	1,785	RBA
1	8	Silver Spring, MD	1992	35	7:00-9:00 a.m.	78	9	13	22	7,080	RBA

Average Pass-By Trip Percentage: 62

“—” means no data were provided

**Table E.38 Pass-By and Non-Pass-By Trips Weekday, PM Peak Period
Land Use Code 945—Gasoline/Service Station with Convenience Market**

SIZE (1,000 SQ. FT. GFA)	VEHICLE FUELING POSITIONS	LOCATION	WEEKDAY SURVEY DATE	NO. OF INTERVIEWS	TIME PERIOD	PASS-BY TRIP (%)	NON-PASS-BY TRIPS (%)			ADJ. STREET PEAK HOUR VOLUME	SOURCE
							PRIMARY	DIVERTED	TOTAL		
0.8	8	Louisville area, KY	1993	83	4:00-6:00 p.m.	52	8	40	48	4,965	Barton- Aschman Assoc.
0.6	8	Louisville, KY	1993	60	4:00-6:00 p.m.	53	20	27	47	1,491	Barton- Aschman Assoc.
0.7	10	Louisville, KY	1993	—	4:00-6:00 p.m.	57	19	24	43	1,812	Barton- Aschman Assoc.
0.7	8	Louisville area, KY	1993	—	4:00-6:00 p.m.	72	7	21	28	2,657	Barton- Aschman Assoc.
0.7	10	Louisville area, KY	1993	—	4:00-6:00 p.m.	55	18	29	45	2,657	Barton- Aschman Assoc.
0.8	8	Silver Spring, MD	1992	36	4:00-6:00 p.m.	67	14	19	33	3,095	RBA
0.4	8	Derwood, MD	1992	46	4:00-6:00 p.m.	46	11	43	54	3,770	RBA
2.1	8	Kensington, MD	1992	31	4:00-6:00 p.m.	52	13	35	48	1,785	RBA
1	8	Silver Spring, MD	1992	35	4:00-6:00 p.m.	54	3	43	46	7,080	RBA

Average Pass-By Trip Percentage: 56

“—” means no data were provided



**Chicago Metropolitan
Agency for Planning**

233 South Wacker Drive
Suite 800
Chicago, Illinois 60606

312 454 0400
www.cmap.illinois.gov

November 20, 2017

Stephen B. Corcoran, P.E., PTOE
Director of Traffic Engineering
Eriksson Engineering Associates, Ltd.
145 Commerce Drive
Grayslake, IL 60030

**Subject: IL 59 (Sutton Road) @ Norwood Lane
IDOT**

Dear Mr. Corcoran:

In response to a request made on your behalf and dated November 18, 2017, we have developed year 2040 average daily traffic (ADT) projections for the subject location.

ROAD SEGMENT	Current ADT	Year 2040 ADT
IL 59-Sutton Rd. @ Norwood Ln	37,700	43,300

Traffic projections are developed using existing ADT data provided in the request letter and the results from the October 2017 CMAP Travel Demand Analysis. The regional travel model uses CMAP 2040 socioeconomic projections and assumes the implementation of the GO TO 2040 Comprehensive Regional Plan for the Northeastern Illinois area.

If you have any questions, please call me at (312) 386-8806.

Sincerely,

Jose Rodriguez, PTP, AICP
Senior Planner, Research & Analysis

cc: Quigley (IDOT)
S:\AdminGroups\ResearchAnalysis\TrafficForecasts_CY2017\Bartlett\du-68-17\du-68-17.docx

Table 4C-1. Warrant 1, Eight-Hour Vehicular Volume

Condition A—Minimum Vehicular Volume

Number of lanes for moving traffic on each approach		Vehicles per hour on major street (total of both approaches)				Vehicles per hour on higher-volume minor-street approach (one direction only)			
Major Street	Minor Street	100% ^a	80% ^b	70% ^c	56% ^d	100% ^a	80% ^b	70% ^c	56% ^d
1	1	500	400	350	280	150	120	105	84
2 or more	1	600	480	420	336	150	120	105	84
2 or more	2 or more	600	480	420	336	200	160	140	112
1	2 or more	500	400	350	280	200	160	140	112

Condition B—Interruption of Continuous Traffic

Number of lanes for moving traffic on each approach		Vehicles per hour on major street (total of both approaches)				Vehicles per hour on higher-volume minor-street approach (one direction only)			
Major Street	Minor Street	100% ^a	80% ^b	70% ^c	56% ^d	100% ^a	80% ^b	70% ^c	56% ^d
1	1	750	600	525	420	75	60	53	42
2 or more	1	900	720	630	504	75	60	53	42
2 or more	2 or more	900	720	630	504	100	80	70	56
1	2 or more	750	600	525	420	100	80	70	56

^a Basic minimum hourly volume

^b Used for combination of Conditions A and B after adequate trial of other remedial measures

^c May be used when the major-street speed exceeds 40 mph or in an isolated community with a population of less than 10,000

^d May be used for combination of Conditions A and B after adequate trial of other remedial measures when the major-street speed exceeds 40 mph or in an isolated community with a population of less than 10,000



Coordinate Collision Diagram Report

1/1/2011 to 12/31/2011

For XCoordinate 2829385.91952772 : YCoordinate 1940658.49080392 | Foot Tolerance : 250 | County : DuPage | Intersection Related: Intersections | *See Notes at End of Report.

Date	Weather	Roadway	Injuries A	Injuries B	C	Killed	Type of Crash	Light Condition	Mile	XCoordinate YCoordinate	Vehicle Type	DIRP	Maneuver	Event 1	Loc 1	Event 2	Loc 2	Event 3	Loc 3	Unit
201101243203																				
7/25/2011 12:40 PM	Clear	Dry	0	2	0	0	Turning	Daylight	33.05	2829386.03450329 1940658.75610259	Passenger	East	Turning Left	Motor Vehicle In Traffic	On Pavement (Roadway)	(UNK)	(UNK)	(UNK)	(UNK)	1
201101387039																				
11/6/2011 6:13 PM	Clear	Dry	0	0	0	0	Turning	Darkness, Lighted Road	33.05	2829386.0303839 1940658.02386321	Van/Mini-Van	North	Straight Ahead	Motor Vehicle In Traffic	On Pavement (Roadway)	(UNK)	(UNK)	(UNK)	(UNK)	2
											Passenger	West	Turning Right	Motor Vehicle In Traffic	On Pavement (Roadway)	(UNK)	(UNK)	(UNK)	(UNK)	1



Coordinate Collision Diagram Report

1/1/2011 to 12/31/2011

For XCoordinate 2829385.91952772 : YCoordinate 1940658.49080392 | Foot Tolerance : 250 | County : DuPage | Intersection Related: Intersections | *See Notes at End of Report.

TOTAL CRASHES	FATAL CRASHES	A INJURY CRASHES	B INJURY CRASHES	C INJURY CRASHES	PROPERTY DAMAGE CRASHES	TOTAL KILLED	TOTAL INJURED	A INJURIES	B INJURIES	C INJURIES
2	0	0	1	0	1	0	2	0	0	2

Type of Crash	Total	%	Day of Wk	Total	%	Hour of Day	Total	%	Vehicle Type	Total	%
Turning	2	100.0%	Monday	1	50.0%	Noon	1	50.0%	Passenger	2	50.0%
TOTAL:	2		Sunday	1	50.0%	6 PM	1	50.0%	Van/Mini-Van	2	50.0%
			TOTAL:	2		TOTAL:	2		TOTAL:	4	

Weather Cond	Total	%	Light Cond	Total	%	Road Surface	Total	%	DIRP	Total	%
Clear	2	100.0%	Darkness, Lighted Road	1	50.0%	Dry	2	100.0%	East	1	25.0%
TOTAL:	2		Daylight	1	50.0%	TOTAL:	2		North	2	50.0%
			TOTAL:	2					West	1	25.0%
									TOTAL:	4	



Coordinate Collision Diagram Report

1/1/2011 to 12/31/2011

For XCoordinate 2829385.91952772 : YCoordinate 1940658.49080392 | Foot Tolerance : 250 | County : DuPage | Intersection Related: Intersections | *See Notes at End of Report.

Notes

DISCLAIMER: The motor vehicle crash data referenced herein was provided by the Illinois Department of Transportation. Any conclusions drawn from analysis of the aforementioned data are the sole responsibility of the data recipient(s). Additionally, for coding years 2015 to present, the Bureau of Data Collection uses the exact latitude/longitude supplied by the investigating law enforcement agency to locate crashes. Therefore, location data may vary in previous years since data prior to 2015 was physically located by bureau personnel.



Coordinate Collision Diagram Report

1/1/2012 to 12/31/2012

For XCoordinate 2829385.91952772 : YCoordinate 1940658.49080392 | Foot Tolerance : 250 | County : DuPage | Intersection Related: Intersections | *See Notes at End of Report.

Date	Weather	Roadway	A	Injuries	B	C	Killed	Type of Crash	Light Condition	Mile	XCoordinate	YCoordinate	Vehicle Type	DIRP	Mansuver	Event 1	Loc 1	Event 2	Loc 2	Event 3	Loc 3	Unit	
201201012445																							
1/2/2012 9:51 AM	Snow	Dry	1	1	3	0	0	Rear End	Daylight	33.14	2829385.9028459	1940658.48625895	Tractor With Semi-Trailer	North	Straight Ahead	Motor Vehicle In Traffic	On Pavement (Roadway)	(UNK)	(UNK)	(UNK)	(UNK)	(UNK)	1
													Passenger	North	Slow/Stop - Right Turn	Motor Vehicle In Traffic	On Pavement (Roadway)	Motor Vehicle In Traffic	On Pavement (Roadway)	(UNK)	(UNK)	(UNK)	2
													SUV	West	Slow/Stop - Left Turn	Motor Vehicle In Traffic	On Pavement (Roadway)	(UNK)	(UNK)	(UNK)	(UNK)	(UNK)	3



Illinois Department of Transportation
Division of Traffic Safety

Report No : SDM-RC001
Sorted by : Mile / Date / ICN

Coordinate Collision Diagram Report

1/1/2012 to 12/31/2012

For XCoordinate 2829385.91952772 : YCoordinate 1940658.49080392 | Foot Tolerance : 250 | County : DuPage | Intersection Related: Intersections | *See Notes at End of Report.

TOTAL CRASHES	FATAL CRASHES	A INJURY CRASHES	B INJURY CRASHES	C INJURY CRASHES	PROPERTY DAMAGE CRASHES	TOTAL KILLED	TOTAL INJURED	A INJURIES	B INJURIES	C INJURIES
1	0	1	0	0	0	0	5	1	1	3

Type of Crash	Total	%	Day of Wk	Total	%	Hour of Day	Total	%	Vehicle Type	Total	%
Rear End	1	100.0%	Monday	1	100.0%	08 AM	1	100.0%	Passenger	1	33.3%
TOTAL:	1		TOTAL:	1		TOTAL:	1		SUV	1	33.3%
									Tractor W/tn Semi-Trailer	1	33.3%
									TOTAL:	3	

Weather Cond	Total	%	Light Cond	Total	%	Road Surface	Total	%	DIRP	Total	%
Snow	1	100.0%	Daylight	1	100.0%	Dry	1	100.0%	North	2	66.7%
TOTAL:	1		TOTAL:	1		TOTAL:	1		West	1	33.3%
									TOTAL:	3	



Coordinate Collision Diagram Report

1/1/2012 to 12/31/2012

For XCoordinate 2829385.91952772 : YCoordinate 1940658.49080392 | Foot Tolerance : 250 | County : DuPage | Intersection Related: Intersections | *See Notes at End of Report.

Notes

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Coordinate Collision Diagram Report

1/1/2013 to 12/31/2013

For XCoordinate 2829385.91952772 : YCoordinate 1940658.49080392 | Foot Tolerance : 250 | County : DuPage | Intersection Related: Intersections | *See Notes at End of Report.

Date	Weather	Roadway	Injuries	A	B	C	Killed	Type of Crash	Light Condition	Mile	XCoordinate	YCoordinate	Vehicle Type	DIRP	Maneuver	Event 1	Loc 1	Event 2	Loc 2	Event 3	Loc 3	Unit	
201301217584																							
10/31/2013 3:50 PM	Rain	Wet	0	0	0	0	0	Rear End	Daylight	33.14	2829385.06692991	1940658.48732455	Truck Single Unit	North	Slow/Stop In Traffic	Motor Vehicle In Traffic	On Pavement (Roadway)	(UNK)	(UNK)	(UNK)	(UNK)	(UNK)	1
													Passenger	North	Slow/Stop In Traffic	Motor Vehicle In Traffic	On Pavement (Roadway)	(UNK)	(UNK)	(UNK)	(UNK)	(UNK)	2



Coordinate Collision Diagram Report

1/1/2013 to 12/31/2013

For XCoordinate 2829385.91952772 : YCoordinate 1940658.49080392 | Foot Tolerance : 250 | County : DuPage | Intersection Related: Intersections | *See Notes at End of Report.

TOTAL CRASHES	FATAL CRASHES	A INJURY CRASHES	B INJURY CRASHES	C INJURY CRASHES	PROPERTY DAMAGE CRASHES	TOTAL KILLED	TOTAL INJURED	A INJURIES	B INJURIES	C INJURIES
1	0	0	0	0	1	0	0	0	0	0

Type of Crash	Total	%	Day of Wk	Hour of Day	Total	%	Vehicle Type	Total	%
Rear End	1	100.0%	Thursday	3 PM	1	100.0%	Passenger	1	50.0%
TOTAL:	1		TOTAL:	TOTAL:	1		Truck Single Unit	1	50.0%
							TOTAL:	2	

Weather Cond	Total	%	Light Cond	Road Surface	Total	%	DIRP	Total	%
Rain	1	100.0%	Daylight	Wet	1	100.0%	North	2	100.0%
TOTAL:	1		TOTAL:	TOTAL:	1		TOTAL:	2	



Coordinate Collision Diagram Report

11/1/2013 to 12/31/2013

For XCoordinate 2829385.91952772 : YCoordinate 1940658.49080392 | Foot Tolerance : 250 | County : DuPage | Intersection Related: Intersections | *See Notes at End of Report.

Notes

DISCLAIMER: The motor vehicle crash data referenced herein was provided by the Illinois Department of Transportation. Any conclusions drawn from analysis of the aforementioned data are the sole responsibility of the data recipient(s). Additionally, for coding years 2015 to present, the Bureau of Data Collection uses the exact latitude/longitude supplied by the investigating law enforcement agency to locate crashes. Therefore, location data may vary in previous years since data prior to 2015 was physically located by bureau personnel.



Coordinate Collision Diagram Report

1/1/2014 to 12/31/2014

For XCoordinate 2829385.91952772 : YCoordinate 1940658.49080392 | Foot Tolerance : 250 | County : DuPage | Intersection Related: Intersections | *See Notes at End of Report.

Date	Weather	Roadway	Injuries A B C	Killed	Type of Crash	Light Condition	Mile	XCoordinate YCoordinate	Vehicle Type	DIRP	Maneuver	Event 1	Loc 1	Event 2	Loc 2	Event 3	Loc 3	Unit	
201400084087																			
1/23/2014 3:14 PM	Clear	Dry	0 0 0	0	Rear End	Daylight	33.14	2829385.63318484 1940658.47965034	Passenger	West	Straight Ahead	Motor Vehicle In Traffic	On Pavement (Roadway)	(UNK)	(UNK)	(UNK)	(UNK)	(UNK)	1
4/11/2014 1:21 PM	Clear	Dry	0 2 0	0	Turning	Daylight	33.14	2829385.63318484 1940658.47965034	SUV	West	Turning Left	Motor Vehicle In Traffic	Intersectio n	(UNK)	(UNK)	(UNK)	(UNK)	(UNK)	1
10/15/2014 8:28 AM	Clear	Dry	0 0 0	0	Turning	Daylight	33.14	2829385.91952772 1940658.49080392	Passenger	North	Straight Ahead	Motor Vehicle In Traffic	Intersectio n	(UNK)	(UNK)	(UNK)	(UNK)	(UNK)	2
12/1/2014 8:03 AM	Clear	Dry	0 0 0	0	Rear End	Daylight	33.14	2829385.63318484 1940658.47965034	Truck Single Unit	North	Straight Ahead	Motor Vehicle In Traffic	On Pavement (Roadway)	(UNK)	(UNK)	(UNK)	(UNK)	(UNK)	2
12/12/2014 6:41 AM	Clear	Ice	0 0 0	0	Rear End	Darkness, Lighted Road	33.14	2829385.63446228 1940658.42382018	Passenger	Unknown	Slow/Stop - Right Turn	Motor Vehicle In Traffic	On Pavement (Roadway)	(UNK)	(UNK)	(UNK)	(UNK)	(UNK)	1
									Passenger	Unknown	Slow/Stop - Right Turn	Motor Vehicle In Traffic	On Pavement (Roadway)	(UNK)	(UNK)	(UNK)	(UNK)	(UNK)	2
									Passenger	South	Slow/Stop In Traffic	Motor Vehicle In Traffic	On Pavement (Roadway)	(UNK)	(UNK)	(UNK)	(UNK)	(UNK)	1
									Passenger	South	Slow/Stop In Traffic	Motor Vehicle In Traffic	On Pavement (Roadway)	(UNK)	(UNK)	(UNK)	(UNK)	(UNK)	2



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TOTAL CRASHES	FATAL CRASHES	A INJURY CRASHES	B INJURY CRASHES	C INJURY CRASHES	PROPERTY DAMAGE CRASHES	TOTAL KILLED	TOTAL INJURED	A INJURIES	B INJURIES	C INJURIES
5	0	0	1	0	4	0	2	0	2	0

Type of Crash	Total	%	Day of Wk	Total	%	Hour of Day	Total	%	Vehicle Type	Total	%
Rear End	3	60.0%	Monday	1	20.0%	06 AM	1	20.0%	Passenger	8	80.0%
Turning	2	40.0%	Thursday	1	20.0%	08 AM	2	40.0%	SUV	1	10.0%
TOTAL:	5		Friday	2	40.0%	1 PM	1	20.0%	Truck Single Unit	1	10.0%
			Sunday	1	20.0%	3 PM	1	20.0%	TOTAL:	10	
			TOTAL:	5		TOTAL:	5				

Weather Cond	Total	%	Light Cond	Total	%	Road Surface	Total	%	DIRP	Total	%
Clear	5	100.0%	Darkness, Lighted Road	1	20.0%	Dry	4	80.0%	North	2	20.0%
TOTAL:	5		Daylight	4	80.0%	Ice	1	20.0%	South	2	20.0%
			TOTAL:	5		TOTAL:	5		Unknown	2	20.0%
									West	4	40.0%
									TOTAL:	10	



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Notes

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Coordinate Collision Diagram Report

1/1/2015 to 12/31/2015

For XCoordinate 2829385.91952772 : YCoordinate 1940658.49080392 | Foot Tolerance : 250 | County : DuPage | Intersection Related: Intersections | *See Notes at End of Report.

Date	Weather	Roadway	Injuries A	Injuries B	C	Killed	Type of Crash	Light Condition	Mile	XCoordinate YCoordinate	Vehicle Type	DIRP	Maneuver	Event 1	Loc 1	Event 2	Loc 2	Event 3	Loc 3	Unit	
2015011375391																					
3/23/2015 10:00 AM	Snow	Snow or Slush	0	0	0	0	Turning	Daylight	33.14	2829404.73063345 1940658.22354284	Van/Mini-Van	West	Slow/Stop In Traffic	Motor Vehicle In Traffic	On Pavement (Roadway)	(UNK)	(UNK)	(UNK)	(UNK)	(UNK)	2
											Passenger	Northeast	Turning Right	Motor Vehicle In Traffic	On Pavement (Roadway)	(UNK)	(UNK)	(UNK)	(UNK)	(UNK)	1
2015011398829																					
5/20/2015 7:31 AM	Clear	Dry	0	0	0	0	Rear End	Daylight	33.14	2829363.22069313 1940658.81755399	Passenger	South	Straight Ahead	Motor Vehicle In Traffic	On Pavement (Roadway)	(UNK)	(UNK)	(UNK)	(UNK)	(UNK)	1
											Pickup	South	Slow/Stop In Traffic	Motor Vehicle In Traffic	On Pavement (Roadway)	(UNK)	(UNK)	(UNK)	(UNK)	(UNK)	2
2015011336648																					
7/10/2015 6:20 AM	Clear	Dry	0	1	0	0	Turning	Daylight	33.14	2829404.69168533 1940658.22201405	Passenger	North	Slow/Stop In Traffic	Motor Vehicle In Traffic	On Pavement (Roadway)	(UNK)	(UNK)	(UNK)	(UNK)	(UNK)	1
											Van/Mini-Van	North	Turning Right	Motor Vehicle In Traffic	On Pavement (Roadway)	(UNK)	(UNK)	(UNK)	(UNK)	(UNK)	2
2015011216663																					
10/6/2015 10:35 AM	Clear	Dry	0	0	0	0	Turning	Daylight	33.14	2829363.1982959 1940658.39520634	Tractor With Semi-Trailer	South	Straight Ahead	Motor Vehicle In Traffic	On Pavement (Roadway)	(UNK)	(UNK)	(UNK)	(UNK)	(UNK)	2
											SUV	Southwest	Turning Left	Motor Vehicle In Traffic	On Pavement (Roadway)	(UNK)	(UNK)	(UNK)	(UNK)	(UNK)	1



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4	0	0	1	0	3	0	1	0	0	1

Type of Crash	Total	%	Day of Wk	Total	%	Hour of Day	Total	%	Vehicle Type	Total	%
Rear End	1	25.0%	Monday	1	25.0%	06 AM	1	25.0%	Passenger	3	37.5%
Turning	3	75.0%	Tuesday	1	25.0%	07 AM	1	25.0%	Pickup	1	12.5%
TOTAL:	4		Wednesday	1	25.0%	10 AM	2	50.0%	SUV	1	12.5%
			Friday	1	25.0%	TOTAL:	4		Tractor With Semi-Trailer	1	12.5%
			TOTAL:	4					Van/Mini-Van	2	25.0%
									TOTAL:	8	

Weather Cond	Total	%	Light Cond	Total	%	Road Surface	Total	%	DIRP	Total	%
Clear	3	75.0%	Daylight	4	100.0%	Dry	3	75.0%	North	2	25.0%
Snow	1	25.0%	TOTAL:	4		Snow or Slush	1	25.0%	Northeast	1	12.5%
TOTAL:	4					TOTAL:	4		South	3	37.5%
									Southwest	1	12.5%
									West	1	12.5%
									TOTAL:	8	



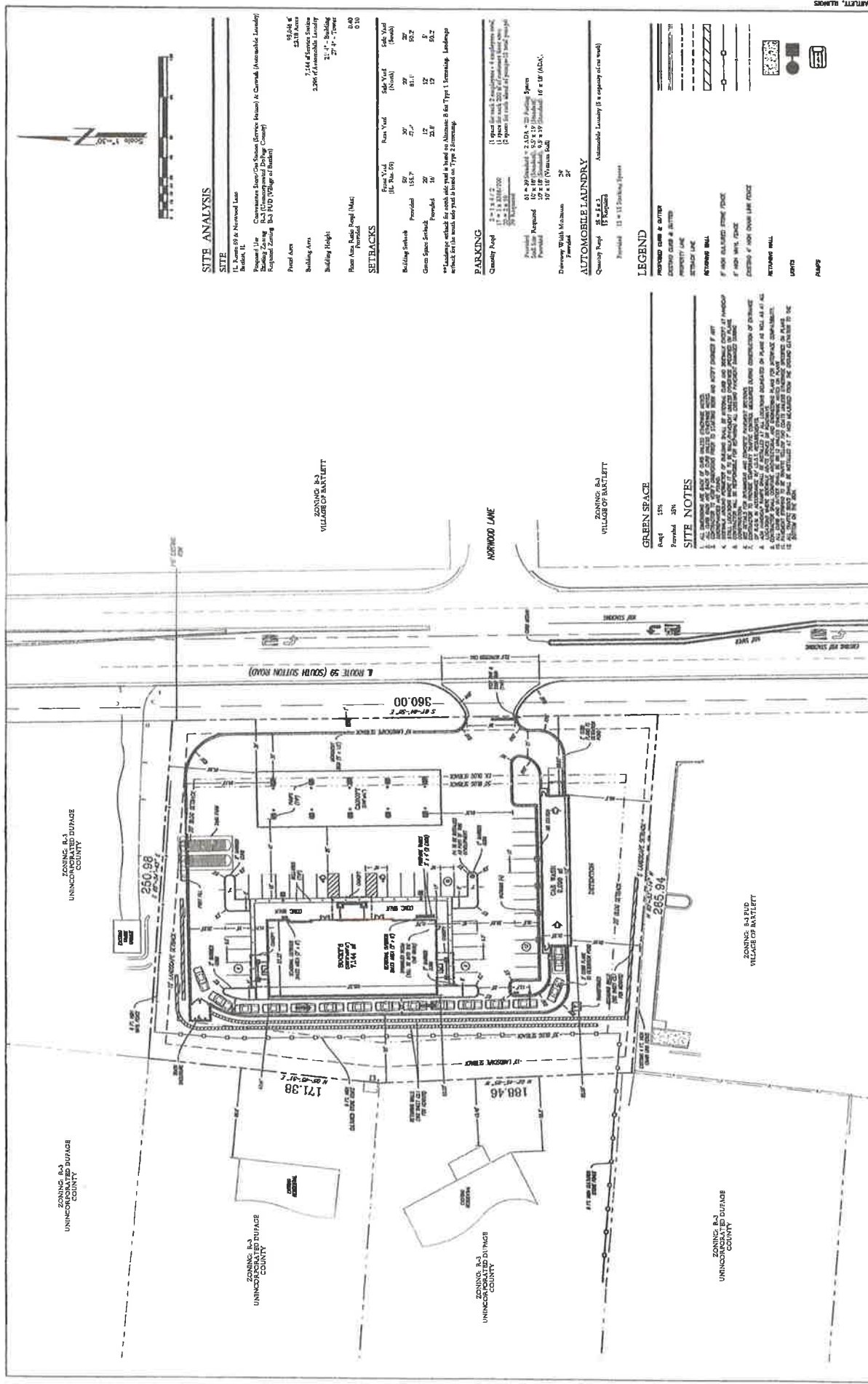
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SITE ANALYSIS

SITE
 IL Route 59 & Norwood Lane
 Bartlett, IL
 Subdivision: None (Site Specific (Electric, Mason) & Curved, (Automobile Laundry)
 Building Zoning: B-3 (Unincorporated DuPage County)
 Required Zoning: B-3 (Village of Bartlett)

Parcel Area 68,046 sq. ft.
Building Area 7,144 sq. Service Station
 2,296 sq. Automobile Laundry
Building Height 27' - 0" (4 stories)
Front Area Ratio Reg'd (Max) 0.10
SETRACKS

Item (V.L.)	Item (V.L.)	Item (V.L.)	Item (V.L.)	Item (V.L.)
(ft. Min. 10)	(ft. Min. 10)	(ft. Min. 10)	(ft. Min. 10)	(ft. Min. 10)
Building Setback	50'	30'	20'	20'
Green Space Setback	155.7'	20'	12'	12'
Front Area Ratio Reg'd (Max)	0.10	0.10	0.10	0.10

*Minimum setback for auto set-back is based on Minimum B for Type 1, Streetwall - Landings setback on the north side of the lot on Type 2 coverage.

PARKING

Quantity	Reg'd	Provided
31	31	31

(1 open for each 2,000 sq. ft. of building area & 1 for each 100 sq. ft. of parking area)
 15 spaces for each level of parking (15 total)

AUTOMOBILE LAUNDRY
 Automobile Laundry (B & capacity of car wash)
 Quantity Reg'd 15 Vehicles
 Provided 15 * 13' Parking Spaces

LEGEND

PROPOSED CURB & GUTTER
 EXISTING CURB & GUTTER
 PROPERTY LINE
 SETBACK LINE
 RETAINING WALL
 FENCE
 FENCE WITH GATE
 DRIVEWAY
 DRIVEWAY WITH GATE
 DRIVEWAY WITH GATE
 DRIVEWAY WITH GATE
 DRIVEWAY WITH GATE

SITE NOTES

- ALL DIMENSIONS ARE BASED ON THE CENTERLINE OF THE DRIVEWAY UNLESS OTHERWISE NOTED.
- EXISTING CURB & GUTTER SHALL BE MAINTAINED UNLESS OTHERWISE NOTED.
- EXISTING DRIVEWAY SHALL BE MAINTAINED UNLESS OTHERWISE NOTED.
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BUCKY'S
 CIVIL ENGINEERS, P.C.
 1110 Commerce Center • Bartlett, IL 60132 • Phone: 630-382-1111 • Fax: 630-382-1112

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SITE PLAN
 ILL. ROUTE 59 & NORWOOD LANE
 BARTLETT, ILLINOIS

C1.1

Project Traffic Review #4

To: Ms. Paula Schumacher
Village Administrator, Village of Bartlett

From: Lynn M. Means, P.E., PTOE
Senior Transportation Engineer

Date: October 29, 2018

Subject: Bartlett Case Number: 18-06
Bucky's Gasoline Station Development
IL Route 59 and Norwood Lane
Bartlett, Illinois

Per your request, Gewalt Hamilton Associates, Inc. (GHA) has reviewed the traffic signal warrant study prepared by Quigg Engineering, Inc. (Quigg) dated July 25, 2018 (marked draft) for the above captioned project. It is our understanding the Quigg study is a supplement to the Traffic Study Memorandum prepared by Eriksson Engineering, Inc. (EEA), with the most recent study dated September 25, 2018.

GHA concurs with the Quigg study's findings that a traffic signal is warranted at the IL Route 59 intersection with Norwood Lane and the proposed site access. Conclusions of this effort and recommendations are presented below:

1. Existing traffic volumes were developed by performing 24-hour traffic volume counts in June 2018 at the IL Route 59 and Norwood Lane intersection. During this timeframe, the IL Route 59 and US Route 20 interchange reconstruction project was ongoing, which has impacted traffic volumes through the subject area. A review of the EEA traffic counts performed in November 2017 are, overall, approximately 15 to 25 percent higher during the weekday peak hours of adjacent street traffic and should, therefore, be used in both studies. *The Quigg study is conservative relative to using lower, June 2018 traffic volumes to satisfy the traffic signal warrant volumes.*
2. The trip generation estimates for the proposed development were generated using the average rates contained within the ITE Trip Generation Handbook, 10th Edition. This is consistent with the methodology contained within EEA Traffic Study. However, as noted in GHA's review of the EEA Traffic Study, the trip generation estimates are approximately 24 to 11 percent lower during the weekday AM and PM peak hours, respectively, when compared to calculations using the ITE equations provided. This represents approximately 136 fewer total trips (68 entering and 68 exiting) during the weekday AM peak and 52 fewer total trips (26 enter and 26 exiting) during the weekday PM peak hour. Accordingly, to help ensure the maximum impacts of the development are considered, it is recommended that the study be revised to utilize the ITE equations. *Again, the Quigg study is conservative relative to using lower, site trip generation volumes to satisfy the traffic signal warrant volumes.*
3. GHA concurs with the Traffic Study pass-by assumptions of 62% during the weekday AM peak hour and 56% during the weekday PM peak hour, representing site-generated vehicle trips that are already present in the adjacent passing stream of traffic. Again, this is consistent with the EEA Traffic Study. However, as previously noted in our past reviews, for Strategic Regional Arterial (SRA) Routes, such as Illinois Route 59, IDOT typically allows only a maximum reduction of 20 percent for pass-by traffic be applied to the development site-generated trips. *The volume of pass-by traffic does not reduce the total trips generated and will still be realized as turning movements at the site access, which will not impact the results of the signal warrant analysis.*

4. Based on our experience with similar gasoline station developments, GHA would expect that a higher percentage of *new* traffic would be of the right-in/right-out type, similar to that assumed in the EEA Traffic Study. *The directional distribution of site traffic will not impact the results of the signal warrant analysis, as the driveway functions as a one-lane approach (combining left- and right-turn exiting volumes).*
5. GHA concurs that the Manual on Uniform Traffic Control Devices (MUTCD) Warrant 1 – Eight Hour Vehicular Volume and Warrant 6 - Coordinated Signal System are satisfied for this location, and have been a basis for traffic signal installation on a SRA Route previously by the Illinois Department of Transportation (IDOT). We also concur that Warrant 7, Crash Experience is not satisfied for this location. *Please confirm that the Quigg study has been submitted to IDOT for review and provide any correspondence or feedback from IDOT pertaining the proposed / requested traffic signal at this intersection.*

We hope you find these comments helpful in your continued assessment of the proposed development. GHA does not believe the comments noted above will materially impact the findings / recommendations of the signal warrant study. Should you have any questions please do not hesitate to contact GHA at 847-478-9700.

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